

AGENDA
FOR THE SPECIAL MEETING OF THE
ACTIVE TRANSPORTATION ADVISORY COMMITTEE

Held electronically via Microsoft Teams
Thursday March 3, 2022 at 4 p.m.

In light of the COVID-19 pandemic and to ensure social distancing, this meeting is closed to the public and can be viewed/heard via MS Teams. Please note that MS Teams callers are identified by their phone number which can be viewed on screen by all attendees at the meeting.

***** Territorial Acknowledgement & Inclusivity Statement *****

- 1. CHAIR'S REMARKS**
- 2. ACTIVE TRANSPORTATION PLAN 2021 REPORT CARD** (attachments)
 - Troy Mckay, Senior Manager of Transportation & Development Services
- 3. SEEKING INPUT ON UPDATING THE ACTIVE TRANSPORTATION PLAN** (attachments)
 - Megan Squires, Senior Transportation Planner
- 4. SHORT-TERM SOLUTIONS TO ADVANCE ACTIVE TRANSPORTATION** (attachments)

* Adjournment *

Next Meeting: March 24, 2022 at 4:00 pm

In order to ensure a quorum, please contact Austin Winters at 250-475-5494 ext. 3508 or
Austin.winters@saanich.ca if you are unable to attend.

Territorial Acknowledgement and Inclusivity Statement

It is appropriate that we begin by acknowledging that the District of Saanich lies within the territories of the ləkʷəŋən (lay-kwung-gen) peoples represented by the Songhees and Esquimalt Nations and the W̱SÁNEĆ (weh-saanich) peoples represented by the Tsartlip (Sart-Lip), Pauquachin (Paw-Qua-Chin), Tsawout (Say-Out), Tseycum (Sigh-Come) and Malahat (Mal-a-hat) Nations.

We are committed to celebrating the rich diversity of people in our community. We are guided by the principle that embracing diversity enriches the lives of all people. We all share the responsibility for creating an equitable and inclusive community and for addressing discrimination in all forms.

Active Transportation Plan Report Card 2021

Through the Moving Saanich Forward project, we have worked with the community to develop a vision along with policies and actions to guide the development of safe, attractive and convenient active transportation options for people of all ages and abilities over the next 30-years.

COVID-19 continues to affect how people work and move in Saanich, and highlights why improvements to active transportation remain a priority.

An important action in the plan is to report annually on our progress. This is our third year of reporting and we have worked hard to fill gaps in our data from our last report card. Generally it appears that residents are doing well in choosing active ways of moving around, however more effort is needed if we are going to achieve all the safety goals set out in the Active Transportation Plan.



Traffic signals upgraded to give more pedestrian priority through Leading Pedestrian Intervals



of bus stops in Saanich now meet BC Transit's accessibility guidelines



of Signals now have audible equipment

Measure of Success	Where we are at	2019 to 2023	How we are doing
General			
People walking	8%	9%	On track
People cycling	5%	6%	On track
People taking transit	10%	12%	On track
ICBC reported vehicle collisions involving people walking (5yr avg)	48	<45	Behind
ICBC reported vehicle collisions involving people cycling (5yr avg)	82	<65	Behind
ICBC reported fatal vehicle collisions involving people walking and cycling (5yr avg)	1	0	Behind



#movingsaanichfwd
OUR 30 YEAR ACTIVE TRANSPORTATION PLAN



Active Transportation Plan Report Card 2021

Measure of Success	Where we are at	2019 to 2023	How we are doing
Connections			
Total length of bicycle network [km]	172	195	Behind
Off-street (AAA)	21	21	On track
Protected on-street (AAA)	4	16	Behind
Neighbourhood Bikeway (AAA)	0	25	Behind
Painted or Buffered Bikelane	117	119	On track
Shared Use Roadways (This number reduces as bike routes are upgraded)	30	12	Behind
Proportion of Saanich's total population within 400 m of the bicycle network	81%*	93%	Behind
Proportion of Saanich's total jobs within 400 m of the bicycle network	89%	90%	On track
Proportion of Saanich's urban land area within 400 m of the bicycle network	81%	80%	Ahead
Total length of sidewalk network [km]	286	306	Behind
Percentage of streets with a sidewalk on at least one side	35%	35%	On track
Percentage of bus stops that are accessible	50%	44%	Ahead
Percentage of bus stops with shelters	36%	35%	Ahead
Convenience			
Percentage of traffic signals with audible pedestrian signals	98%	75%	Ahead
Percentage of traffic signals with pedestrian countdown timers	99%	100%	On track
Percentage of curb ramps that meet Saanich's requirements for accessibility	44%	51%	Behind
Percentage of signals with cyclist detection	37%	40%	On track
Percentage of District owned and operated locations with short-term and long-term bicycle parking and end-of-trip facilities	15%	30%	Behind
Culture			
Number of schools within Saanich that have completed Active and Safe Routes to School Programs in the last 10 years	25	30	On track
Number of public wayfinding displays	22	26	Behind
Number of annual walking and cycling events including infrastructure grand openings	6	4	Ahead

* Improved GIS analysis has refined this value downward

Active Transportation Plan Update

Terms of Reference

DRAFT February 22, 2022

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1. OVERVIEW

Investments in walking, cycling and other forms of active transportation lead to more efficient spending on infrastructure, as well as a more accessible and equitable transportation system overall. A balanced transportation system can also result in significant quality of life, health, safety and economic benefits, which improve the lives of Saanich residents.

The District's first Active Transportation Plan, *Moving Saanich Forward* is a guide for investment in active transportation improvements for the next 30 years. The award-winning plan establishes a vision, goals and targets. It also outlines strategies and actions to ensure progress on the District's policies, standards, infrastructure and programs for active transportation.

In 2023 the ATP will be five years old and five years of implementation will have occurred. The purpose of this update is to work with the community to review and update specific aspects of the plan, and incorporate new and emerging information to ensure that the plan continues to be relevant and actionable for the next five years, and beyond.

COVID-19

In March 2020, the outbreak of the COVID-19 virus was declared a pandemic by the Director-General of the World Health Organization. The Province of BC has implemented a plan to manage the impact of COVID-19 and Saanich has established its own emergency planning and communications, focussing on containment and information activity while preparing for other possible eventualities. As the COVID-19 pandemic evolves, the District of Saanich continues to focus on helping our community stay safe and healthy.

The pandemic impacts the update of the ATP in two important ways. Firstly, the updated plan will need to consider the impacts of the pandemic on active transportation, including the travel patterns and behaviours of residents, as well as road safety. Secondly, the update will take into account different requirements for engaging the public and stakeholders as a result of public health measures. This Terms of Reference outlines an engagement approach that recognizes these requirements and integrates a level of flexibility to respond to the fluidity of the situation.

Equity Lens

Equity refers to the fairness with which impacts, both benefits and costs are distributed. Equity is a key consideration in transportation planning, as the allocation of public resources can significantly impact residents' quality of life, including the livability of their communities and their ability to access services and economic opportunities.

Staff will apply an equity lens to identify applicable data, remove barriers to engagement, and evaluate the impacts of different targets, actions, and priorities for implementation. It is expected that this type of analysis will lead to a more equitable plan and improved active transportation outcomes for Saanich residents in the long-term.

2. PURPOSE

The purpose of the ATP update is to provide a current framework to support implementation of active transportation policies and actions. The updated Plan will guide investment and decision-making for active transportation in the next five years, and beyond.

Fundamental to this plan update will be to ensure alignment with current policy documents and key directions, including recent strategies and plans for climate change, active transportation, and road safety. Progress towards achieving the vision, goals and targets of the ATP will be reviewed, as will select strategies and actions, and the implementation plan. Current demographic and travel data, as well as collision data will form the basis for this update and will provide the evidence to support revised and/or new targets and actions.

3. PROJECT OBJECTIVES

The objectives of the plan update are:

- To update the ATP using current data and policy to inform revised and new targets, and strategies and actions, as well as the implementation plan;
- To address new and emerging issues and technologies related to road safety and electric mobility devices;
- To apply an equity lens to the planning process and updated ATP;
- To use a variety of methods and strategies to communicate and engage with governments, stakeholders and the public on the plan update; and
- To produce an updated plan to guide implementation of active transportation investment, policy, infrastructure and programs for the next five years and beyond.

4. SCOPE

The scope of the ATP update process will be guided by existing and new District-wide policy documents, research into best practices, and input gathered through engagement with governments, stakeholders and the public. The update will focus on the following key areas:

A. Build on and Update Content in the Existing ATP

The existing ATP will form the basis for the update. While the current plan is still relevant and actionable, there are specific aspects that need to be reviewed and updated, based on new and emerging information. The following aspects of the current plan will be updated:

- Data related to demographics, travel patterns and collisions;
- Policy framework;
- Select targets, strategies and actions;
- The implementation plan, including prioritization of actions; and
- Mapping.

B. Address new emerging issues

The updated Plan will examine new and emerging issues. Through a review of current policy documents, research into best practices and based on government, stakeholder and community input, the following issues will be addressed:

- Road safety, including Vision Zero and a Safe Systems approach;
- Electric bicycles and e-mobility devices; and
- Equity and inclusion.

5. TIMELINE

The ATP update is anticipated to take 12 months to complete. Figure 1 provides an overview of the process and Table 1 outlines the planning process showing project phases, key activities and deliverables. Project activities are mostly set, but engagement activities may be adjusted based on the feedback of the Active Transportation Advisory Committee (ATAC) and other governments and community stakeholders. As well, the uncertainty of the pandemic and future public health restrictions may require adjustments to engagement activities. If adjustments are required, staff will make every effort to maintain project timelines and objectives.

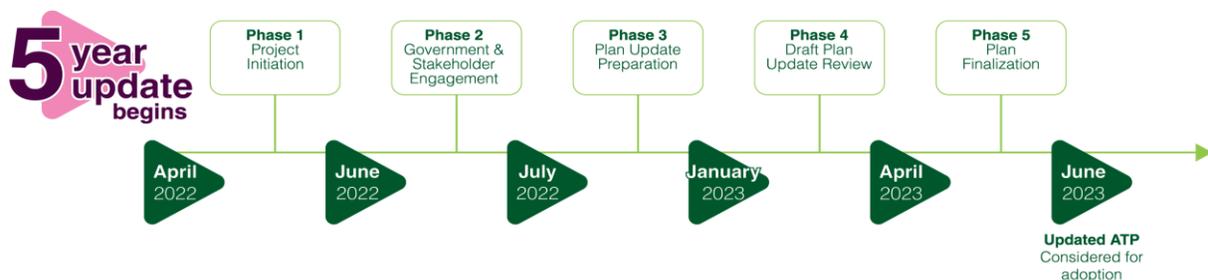


Figure 1: Project Timeline

Table 1: Planning Process Including Phasing, Activities and Deliverables

PHASE	ACTIVITIES	DELIVERABLES
<p>Phase 1</p> <p>Project Initiation</p> <p>April - May 2022</p>	<ul style="list-style-type: none"> • Collect relevant background information and current data on demographics, travel patterns and collisions • Analyse key issues and opportunities, and conduct best practices research <p>Engagement Preparation</p> <ul style="list-style-type: none"> • Develop the project website • Collaborate with the ATAC to finalize the engagement strategy and phase 2 engagement • Prepare engagement and communication materials 	<ul style="list-style-type: none"> • Communications materials and tools (e.g. backgrounder with updated data, website) • A confirmed list of governments and stakeholders • Engagement Strategy

<p>Phase 2</p> <p>Government and Stakeholder Engagement</p> <p>June 2022</p>	<ul style="list-style-type: none"> Organize and implement engagement initiatives that could include a survey and online interviews and meetings Report back on engagement Collaborate with the ATAC to design engagement in phase 4 	<ul style="list-style-type: none"> Engagement and communications materials Information to inform preparation of the draft update of the ATP Engagement summary report A final engagement strategy for phase 4
<p>Phase 3</p> <p>Plan Update Preparation</p> <p>July – December 2022</p>	<ul style="list-style-type: none"> Undertake research and technical analysis, as needed Develop the draft update of the ATP <p>November 2022</p> <ul style="list-style-type: none"> Additional government and stakeholder engagement 	<ul style="list-style-type: none"> Draft update of the ATP Engagement and communications materials Engagement summary report
<p>Phase 4</p> <p>Review of the Draft Updated ATP</p> <p>January 2023 - March 2023</p>	<ul style="list-style-type: none"> Prepare engagement and communication materials Seek feedback and assess support for the draft updated ATP. Organize and implement engagement initiatives that could include a survey, online or in-person events, and online meetings, presentations and/or displays to engage the public. Engage with the ATAC on the review of the draft updated plan 	<ul style="list-style-type: none"> Input on the Draft Updated ATP to inform the final Draft Plan Engagement summary
<p>Phase 5</p> <p>Finalize Draft of the Updated ATP</p> <p>April-June 2023</p>	<ul style="list-style-type: none"> Finalize draft updated plan based on feedback Prepare staff report and presentation Council meeting/adoption 	<ul style="list-style-type: none"> Approved Updated ATP

6. COMMUNITY ENGAGEMENT

Community engagement and participation is an important part of the ATP update planning process. While Council has final authority over adoption of the Plan, a strong level of community input and ownership is key. The planning process will seek to be inclusive and reach a broad spectrum of the community.

Community members can participate in the planning process for the ATP update in a number of ways, including:

- Being a member of the Active Transportation Advisory Committee;

- Participating in engagement events that may be online or in person and may include meetings, interviews, open houses, workshops, walkabouts, displays and presentations.
- Completing surveys;
- Sharing ideas and thoughts with staff; and
- Sharing ideas and thoughts with Council when the plan is presented.

Saanich adheres to the International Association of Public Participation (IAP2) spectrum of participation which identifies the level of community involvement in decision making (Appendix A). Examples of the type of public engagement anticipated for each project phase and the level of public participation are identified in the Table 2 below.

Table 2: Level of Public Engagement for Each Project Phase

Phase	IAP2 Level of Public Participation	Engagement Techniques	Primary Engagement Goal
All Phases	Consult, involve, collaborate	<ul style="list-style-type: none"> • ATAC meetings 	To obtain advice, ideas and feedback throughout the project
Phase 1 Project Initiation	Inform and consult	<ul style="list-style-type: none"> • Communications materials about the project shared via the project website, email and social/print media • Online meetings 	To raise awareness of the project and build an information base to support the project
Phase 2 Government and Stakeholder Engagement	Inform, consult and involve	<ul style="list-style-type: none"> • Information about the project shared by email • Online meetings • Survey 	To continue to raise awareness of the project, share updated information, understand issues and discuss opportunities
Phase 3 Plan Update Preparation	Involve, consult and collaborate	<ul style="list-style-type: none"> • Meetings and workshops (online or in person) • Email 	To work with governments and stakeholders to revise and develop new ideas and solutions
Phase 4 Review of the Draft Updated ATP	Inform, consult and involve	<ul style="list-style-type: none"> • Information and engagement opportunities shared via the project website, email, and social/print media • Meetings & presentations (online or in person) • Displays & open houses (online or in person) • Survey 	To receive community feedback on the draft updated ATP

Phase 5 Finalize Draft of Updated ATP	Inform	<ul style="list-style-type: none"> Information shared via the project website, email, and social/ print media 	To inform the public, governments and stakeholders of the final draft updated ATP
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7. ACTIVE TRANSPORTATION ADVISORY COMMITTEE

The Active Transportation Advisory Committee (ATAC) will provide advice to the Project Team and act as community liaison throughout the planning process. In accordance with its approved Terms of Reference, the ATAC is comprised of a maximum of 11 individuals who are qualified in a discipline relevant to the committee mandate or who have a perspective necessary for fulsome consideration of the associated topic (Appendix B). Preference is given to a membership that is representative of the following areas:

- One member of Council to serve as Chair, appointed by the Mayor;
- A member of the Parent Advisory Group, preferably with experience in Safe Routes to School programming;
- A member with expertise in mobility of people with disabilities;
- A member with transit experience;
- A member from University of Victoria or Camosun College staff related to Active Transportation;
- A member from a cycling advocacy group;
- A member from a pedestrian advocacy group; and
- One youth voting member described as 16-24 years of age.

Key roles of the ATAC as part of the ATP update planning process will be to:

- Advise on public engagement to ensure broad outreach is effectively undertaken with stakeholders and residents;
- Promote the project and public engagement opportunities within the community;
- Provide support to staff at public engagement events; and
- Provide feedback to staff on content and the draft updated plan.

In addition to the ATAC, smaller technical groups may be assembled to address topic specific issues as they arise. The technical working groups could include other Saanich Departments, BC Transit, the Capital Regional District or other key stakeholders.

8. STAKEHOLDERS

A list of stakeholders is outlined in Appendix C. This list may change once the planning process gets underway. The identified stakeholder groups include:

- Government
- Saanich departments

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- Community Associations
 - External agencies
 - School districts, universities and colleges
 - Health agencies
 - Economic development organizations
 - Development industry
 - Transportation advocacy

9. ROLES AND RESPONSIBILITIES

Staff

This process will be led by the Project Planner with support from other departments and consultants as required. Staff will be responsible for project management, engagement, data analysis, and producing and recommending an updated ATP for Council's consideration. The Project Team is made up of the Project Planner, Engineering staff, and departmental representatives from Planning, Parks, Finance, and Corporate Communications, along with other Saanich staff as needed.

Active Transportation Advisory Committee

The Advisory Committee's role is to provide advice to the Project Team and act as community liaison throughout the planning process.

First Nations

The District of Saanich is committed to developing a strong and fair, government-to-government relationship with Indigenous peoples that is based on respect, cooperation and partnership to address shared interests and priorities. The District will engage with First Nations to seek input and review the draft updated ATP.

External Stakeholders

External stakeholders such as Community Associations, agencies, schools (including public and private schools, universities and colleges), economic and land development organizations, and advocacy organizations will contribute in the same way as the public, but may also be the subject of focused consultation with respect to their particular areas of interest. Stakeholder groups can also help with outreach, identifying issues, opportunities and actions, and reviewing policy options.

The General Public

All individuals having an interest in active transportation will be invited to participate in the planning process to provide input and review the draft updated ATP.

Consultants

Consultants may be retained to provide process support or undertake studies or research to supplement the knowledge of the Project Team.

Council

Council is responsible for allocating project resources, setting priorities, and making decisions related to approval of the updated ATP. Saanich Council has final authority for adoption of the plan.

10. RELATIONSHIP TO OTHER INITIATIVES

A key element of the ATP update will be to ensure that the plan is well-integrated and aligned with other Saanich and regional initiatives currently in progress, or completed in the last five years. Initiatives that could influence the ATP update include, but are not limited to:

A. Saanich Initiatives

Housing Strategy (2021) – The Housing Strategy is a 10-year framework that provides direction on how Saanich can achieve greater housing supply, affordability and diversity, as well as accommodate a broad range of community housing needs now and in the future. The Strategy acknowledges that overall household affordability is dependent on the combined costs of housing and transportation. Transportation can be a major household expense depending on where a person lives and what modes of transportation are available to them.

Climate Plan – 100% Renewable & Resilient Saanich (2020) – The approved Climate Plan highlights the current climate emergency; provides a vision and lays out goals, strategies and targets for Saanich to respond quickly and effectively to this global crisis. Investment in active transportation is a key priority to enable Saanich to respond quickly and effectively to the climate emergency.

Electric Mobility Strategy (2020) – The approved Strategy outlines a rapid transition to light duty electric vehicles and electric bikes in Saanich. It contains 38 actions in five focus areas. The actions support key climate targets including 22% of trips by active transportation by 2030 and 30% of trips by active transportation by 2050.

Local Area Plans and Centre, Corridor and Village Plans – The plans are all part of the Official Community Plan and include transportation and mobility direction, including policies for active transportation. The Cordova Bay Local Area Plan is complete and scheduled for Public Hearing in early 2022. The Cadboro Bay Local Area Plan is nearing completion and will likely go to Council in 2022. Council has endorsed the Uptown-Douglas Plan and it schedule for public hearing in February 2022.

B. Provincial and Regional Initiatives

Active Transportation Strategy ‘Move, Commute, Connect’ (2019) - As part of the CleanBC plan, this strategy aims to double the percentage of trips taken by active transportation by 2030, as well as inspire British Columbians to choose active transportation, and support communities to build integrated and accessible active transportation networks. The strategy is part of a provincial effort to meet legislated targets of reduced greenhouse gas emissions.

Active Transportation Design Guide (2019) – Working in conjunction with the Active Transportation Strategy, the Design Guide is a comprehensive set of planning and engineering guidelines that offer recommendations for selection, design, implementation and maintenance of active transportation infrastructure in British Columbia.

South Island Transportation Strategy (2020) - This strategy lays the groundwork for future improvements to the ways people get around southern Vancouver Island. The main focus is on transit and active transportation.

City of Victoria's Sustainable Mobility Strategy (2019) - This strategy takes a systems-approach and outlines the City's mobility values, policy positions and key initiatives.

District of Central Saanich Active Transportation Plan (2021) - This plan includes policies to further plan goals which are to: a) increase walking and cycling; b) improve transit; c) create safer streets; and 4) celebrate local character.

Township of Esquimalt Active Transportation Network Plan (currently in phase 3 of the planning process) - Once completed this plan will identify gaps in the existing pedestrian and cycling network and provide a roadmap for improvements in the short (5 years), medium (10 years) and long-term (11+ years).

11. BUDGET

In addition to staff resources, the budget for the project is \$150,000. The budget for the project includes costs for technical consulting services, government, stakeholder and public engagement, production and graphic design work, and contingency. The project costs have been approved in the Saanich 2021 budget.

12. DELIVERABLE

The updated ATP will be the primary deliverable of this planning process.

APPENDIX A – Adaptation of the IAP2 Spectrum of Public Participation

	Intensity of Engagement				
	Inform	Consult	Involve	Collaborate	Empower
Public Participation Goal	To provide the public with balanced and objective information to assist them in understanding the problem, alternatives and/or solutions	To obtain public feedback on analysis, alternatives and/or decision	To work directly with the public throughout the process to ensure that public concerns and aspirations are consistently understood and considered	To partner with the public in each aspect of the decision including the development of alternatives and the identification of the preferred solution	To place final decision-making in the hands of the public
Promise to the Public	We will keep you informed	We will keep you informed, listen to and acknowledge concerns and aspirations, and provide feedback on how public input influenced the decision	We will work with you to ensure that your concerns and aspirations are directly reflected in the alternatives developed and provide feedback on how public input influenced the decision	We will look to you for advice and innovation in formulating solutions and incorporate your advice and recommendations into the decisions to the maximum extent possible	We will implement what you decide

APPENDIX B – Saanich ATAC Terms of Reference

Definition of Active Transportation

Active Transportation is any form of human-powered transportation, including walking, cycling, public transit, roller-blading, skateboarding and moving with mobility devices (including power wheelchairs and mobility scooters).

Purpose

The purpose of the Active Transportation Advisory Committee (ATAC) is to provide advice and recommendations to District of Saanich Council. Discussion will focus on achieving the District's goals of increasing active transportation mode share, working towards zero traffic related fatalities and serious injuries, and reducing greenhouse gas emissions. Discussion topics will include those related to policy, pedestrian and cycling networks, access to transit, environmental and health considerations, regional connectivity, safety and suggesting amenities and design specifications to deliver a successful Active Transportation program in Saanich. The committee will also help identify and implement programs to influence individual decisions to promote Active Transportation.

Mandate

The Active Transportation Advisory Committee will, consistent with the purpose described above, undertake the following:

- Identify strategies that work towards the District of Saanich's quantifiable goals for:
 - Increasing Active Transportation mode share
 - Reducing overall travel demand on Saanich roadways, as measured by vehicle kilometres traveled (VKT)
 - Working towards zero traffic related fatalities or serious injuries on Saanich roads
- Increase awareness of Active Transportation in Saanich through promotion, education and community outreach.
- Provide advice to Council on projects, policies or bylaws under consideration by staff or Council, and on additional topics of interest to committee members that fall within the subject area of the committee.
- Provide advice and leadership on citizen and stakeholder engagement related to Active Transportation.
- Raise awareness of active transportation as a healthy and environmentally sustainable activity.
- Improve the quality of Active Transportation in Saanich with recommendations and support of safe, innovative, and cost effective design principles.
- Encourage land use patterns that support active modes of transportation.
- Promote integrated and connected Active Transportation Policy and networks across the Greater Victoria Region.

-
- Periodically report to Council on the progress and success of Active Transportation in Saanich.
 - Respond to Council requests for advice and information on Active Transportation in Saanich.
 - Review and provide feedback on the Strategic Plan and annual Capital Plan.
- Any initiatives proposed by the committee that has resource implications, including staff time, would have to be approved by Council.

Meetings

The Committee will meet a minimum of four times per year in accordance with its regular schedule of meetings established annually at the first meeting of the year. No meetings are held during the summer and winter breaks (July, August and December). Special meetings may be held at the call of the Chair. The meeting rules and procedures will be in accordance with the Council Procedure Bylaw.

Membership

To the extent possible, Advisory Committees will have a diverse membership with respect to gender, age and cultural-ethnic background.

The committee will consist of eleven (11) members qualified in the discipline relevant to the committee mandate or with a perspective necessary for fulsome consideration of the associated topic, with preference given to some specific areas including:

- One member of Council to serve as Chair, appointed by the Mayor;
- A member of the Parent Advisory Group, preferably with experience in Safe Routes to School programming;
- A member with expertise in mobility of people with disabilities;
- A member with transit experience;
- A member from University of Victoria or Camosun College staff related to Active Transportation;
- A member from a cycling advocacy group;
- A member from a pedestrian advocacy group; and
- One youth voting member described as 16-24 years of age.

Members may serve a maximum of seven years on an Advisory Committee, consisting of a one-year term followed by a potential for three, two-year terms. Members who wish to be re-appointed after the completion of a term must re-apply and provide the application and their resume for Council's consideration.

Island Health Authority may assign a trustee to the Committee as a non-voting liaison.

Staff Support

The Engineering Department is the primary contact and together with the Parks and Recreation, Planning, and Police Departments will provide the required professional support. The Legislative Division will provide secretarial and administrative support.

Review of Terms of Reference

These Terms of Reference will be reviewed during the committee's first meeting each term.

DRAFT

APPENDIX C - Stakeholder List

GOVERNMENT	
Mayor and Council / Advisory Committees	<ul style="list-style-type: none"> • Mayor and Council • Planning, Transportation and Economic Development Advisory Committee (PTED) • Healthy Saanich Advisory Committee (HSAC) • Parks, Trails and Recreation Advisory Committee (PTR)
First Nations	<ul style="list-style-type: none"> • Songhees and Esquimalt Nations • Tsartlip, Pauquachin, Tsawout, Tseycum, and Malahat Nations
Neighbouring Municipalities	<ul style="list-style-type: none"> • Victoria, Esquimalt, Oak Bay, Central Saanich
INTERNAL STAKEHOLDERS	
Saanich Departments	<ul style="list-style-type: none"> • Engineering, Planning, Parks, Recreation and Community Services, Finance, Corporate Services, Building, Bylaw, Licensing and Legal Services, Fire, Police
EXTERNAL STAKEHOLDERS	
Community Associations	<ul style="list-style-type: none"> • Neighbourhood Community Associations • Saanich Community Association Network (SCAN)
Agencies	<ul style="list-style-type: none"> • BC Transit • Ministry of Transportation and Infrastructure • Capital Regional District • ICBC • BC Emergency Health Services • BC Paraplegic Association
School Districts, Universities and Colleges	<ul style="list-style-type: none"> • Schools in School District (SD) 61 and 63 • Public school administrations and Parent Advisory Committees • Private schools • University of Victoria and Camosun College
Health Agencies	<ul style="list-style-type: none"> • Vancouver Island Health Authority – Public Health • BC Lung Association • Heart and Stroke Foundation • Canadian National Institute for the Blind • Institute of Aging and Lifelong Health
Economic Development	<ul style="list-style-type: none"> • Greater Victoria Chamber of Commerce • Tourism BC
Development Industry	<ul style="list-style-type: none"> • Urban Development Institute
Advocacy Groups	<ul style="list-style-type: none"> • Capital Bike • Walk On Victoria • Greater Victoria Transit Alliance • Victoria Placemaking Network • Capital Region Equestrians

Memo

To: Active transportation Transportation Advisory Committee (ATAC)
From: Councillor Rebecca Mersereau, Chair
Date: 17 February, 2022
Subject: Identifying short-term/quick-build approaches to improve road safety and active transportation mode share

BACKGROUND

At a meeting on September 27, 2021, Saanich Council passed the following motion:

That Council ask the Active Transportation Advisory Committee to consider and recommend short term and/or pilot projects to improve road safety and active transportation within the District of Saanich where active transportation projects are determined medium to long term priority areas.

This motion resulted from persistent and ongoing calls for action to improve road safety in areas throughout the municipality, most notably at the time this motion was passed, on Tillicum Road and in Rural Saanich. Following a subsequent presentation from Livable Roads for Rural Saanich, ATAC struck a working group to specifically consider the issue of speed limits in Rural Saanich. The Working Group will provide an update at the February 24 ATAC meeting. This report is intended to respond to the above motion from Council in a more comprehensive way, recognizing the District is poised to move forward on a Speed Limit Policy (pending Council approval).

Saanich's 2018 [Active Transportation Plan](#) (the ATP) is the vehicle through which projects are identified and prioritized. The ATP uses a phased approach with short-term priorities identified for years 1-5, medium-term actions for years 5-15, and long-term actions for years 15-30. With the short-term priorities nearing completion, Saanich staff will be undertaking an update of the ATP to identify new priorities for the next five years, and adjusting other Plan elements as appropriate. The Active Transportation Advisory Committee (ATAC) and the community at large will inform this work through planned engagement that will get underway in May.

It should also be noted that Council directed staff to report back on how to expedite the Active Transportation Plan on January 10th, 2022. This work is in progress and a report back to Council is expected by the fall of 2022.

Notwithstanding this work underway, it is important to consider options to improve conditions for active transportation that can be implemented relatively quickly and inexpensively, for areas not addressed in the short-term through the comprehensive approaches envisioned by the ATP. These approaches typically require considerable time and resources dedicated to planning, design, and construction of facilities to achieve durable and high-quality outcomes (e.g., the complete street approaches used for Cook Street, Larchwood Drive, and Shelbourne Street).

DISCUSSION

Strategies to improve active transportation mode shift often fall into these categories: education, enforcement, regulation/policies, and infrastructure. Changing regulations/policies and pursuing change through education tend to be time consuming, and increasing enforcement of existing traffic laws has many limitations. Therefore, it is expected that ATAC's consideration of 'short-term options' will focus on infrastructure changes, both large and small, that could potentially be made in the short term (i.e, months rather than years). Examples may include:

- road diets (i.e., removing vehicle lanes from a roadway and reallocating the extra space for other uses or travelling modes; for more information see <https://www.strongtowns.org/journal/2019/1/23/iowa-dot-helps-educate>),
- traffic calming measures (e.g., bollards, planters, paint, centre-line delineators),
- centre-line hardening at intersections, and
- removal of on-street parking on residential streets to create temporary pedestrian walkways on existing asphalt.

ATAC members are asked to come to the meeting prepared to contribute ideas to a discussion about tools and strategies Saanich could use to improve conditions for active transportation use in the short term.

NEXT STEPS

In the interest of being responsive to Council's motion from October of 2021 and consistent with the spirit of short-term active transportation improvements, ATAC should endeavour to provide advice to Council through a motion as soon as possible. With the approval of the 2022 budget approaching in April, the window is closing for recommendations that may have budget implications this year. Depending on how the discussion unfolds at the February 24 meeting, it may be helpful to strike a working group consisting of a subset of ATAC members to further explore this topic with staff and report back at a subsequent meeting.

Prepared by

Rebecca Mersereau
Councillor & Chair of the Active Transportation Committee

Memo

To: Active transportation Transportation Advisory Committee (ATAC)
From: Councillor Rebecca Mersereau, Chair
Date: 2 March, 2022
Subject: Supplemental information about candidate short-term/quick-build projects

BACKGROUND

At a meeting on September 27, 2021, Saanich Council passed the following motion:

That Council ask the Active Transportation Advisory Committee [ATAC] to consider and recommend short term and/or pilot projects to improve road safety and active transportation within the District of Saanich where active transportation projects are determined medium to long term priority areas.

At the ATAC meeting on February 24, Committee members developed a list of ideas that will be further discussed at a special meeting scheduled for March 3. To help advance the discussion, staff have provided supplemental information about the ideas identified at the Feb. 27 meeting. The supplemental information below consists of suggestions for the number of treatments/installations that would be viable for implementation by the end of 2022, estimated costs, and in some cases criteria to consider in implementing the measures discussed.

SUPPLEMENTAL INFORMATION ABOUT SHORT-TERM PROJECT/PILOT IDEAS

1. Install centerline hardening improvements and pedestrian space delineation at 10 un-signalized crossings (i.e. where there are no traffic lights), focusing on crossings with a history of reported crashes or near misses or are located on higher volume or higher speed roads. Typically these roads and crossings will be 2 lane roads with no centre medians and crossings already equipped with flashing beacons. An example raised by the committee is the crossing of the Colquitz River Trail of Interurban Road, south of Marigold Road.

This work has an estimated cost of \$100k

2. Re-allocate the road space on Quadra Street from Chatterton Way to Tolmie Ave and on Tillicum Road from Arena Road to the Saanich Boundary with Esquimalt to remove general purpose vehicle lanes and install protected cycling lanes using quick build and lower cost techniques. It is understood that these changes could have a negative impact on traffic / transit operations along the corridor, and potentially traffic volumes on nearby streets. A corresponding public information / education campaign should take place at the time of implementation to inform stakeholders and public of the change and share information about the community benefits of these changes. The public information campaign should also

Preet Chaggar

From: Ryan Nicoll [REDACTED]
Sent: Sunday, February 27, 2022 11:09 PM
To: Colin Plant; Karen Harper; Mayor; Nathalie Chambers; Ned Taylor; Susan Brice; Zac De Vries; Council; Judy Brownoff; Rebecca Mersereau
Cc: [REDACTED]
Subject: (External Email) Feedback on Road Safety Strategy

This email sent from outside the District of Saanich. Use caution if message is unexpected or sender is not known to you.

Hello Mayor and Council Members,

Thanks in advance for considering my input on the Road Safety Strategy. I have two brief thoughts that I hope you will consider.

1. Road Painted School Zone Signs

I borrowed this image from Colin Plant's twitter post September 2021 taken on Royal Oak Avenue in front of Lochside Elementary School. This is an excellent and low cost way to improve automobile driver awareness of a school zone. These road painted signs should be used next to all marked school zone signs in Saanich, regardless if there is a required 30km/hr school speed zone limit.



For example, Cloverdale Elementary is located next to Quadra and Cook street. There are already marked school zone signs posted on Quadra and Cook street, near the school, but unfortunately, no required 30km/hr speed limit. A road painted school zone sign like the example above would help significantly increase awareness of the proximity of schools and help vulnerable kids walking and rolling to school. This is especially

needed for Cloverdale Elementary. Road paint is something Saanich can do immediately. Thanks for the idea, Colin!

2. Dedicated Saanich staff positions for traffic calming

I recently noticed a new stop sign on a very short street segment on Leslie Drive at Savannah ave in Quadra Cedar Hill. This short part of Leslie street acts as one of the main entrances to Tolmie Park. On sunny and warm days there are many adults and children walking and rolling to and from the park via Leslie and Savannah. (On a recent sunny day this February, I counted 40 adults and children at the park!) However, the visibility of the intersection of Leslie and Savannah is poor because of large bushes and parked cars as indicated in the picture below. Presumably, the new stop sign was added to help encourage people walking and rolling to stop and pay attention to cross-vehicle traffic on Savannah. But a stop sign does nothing to help vulnerable road users from a distracted or speeding driver.



A much safer solution for all road users would be to introduce some form of traffic calming at this intersection instead of a stop sign. One example could be a temporary quick-build traffic circle like the example shown below. By forcing automobile traffic to slow down at what is a potentially dangerous intersection, safety is increased for all road users. This also assures vulnerable road users and may further increase walking and rolling traffic to the park.



Another example of an opportunity for traffic calming like this is at the intersection of Rainbow and Sevenoaks next to Rainbow park. I am sure there are many other opportunities for traffic calming that could substantially help safety for all road users in our communities.

New dedicated Saanich staff positions focused on traffic calming will reveal more opportunities to help increase and prioritize safety for all members of our community.

Thanks for considering my thoughts and good luck in the upcoming meeting.

-Ryan Nicoll

Support for Item F.2

Dear Saanich Council,

We, at Capital Bike, are happy to see Saanich embarking on not only a new Road Safety Plan but also a Speed Limit Policy and revised Active Transportation Plan. These are key pieces for Saanich to meet its larger safety and climate goals. Adopting Vision Zero/Sustainable Safety will help catapult Saanich into a leading municipality in the province, as will actions to reduce speed limits, whether through MVA changes we are both working on or signage in the shorter term.

One piece that seems to be missing from the current set of plans is that of a Traffic Calming Strategy. Although it could be considered part of a larger Road Safety Strategy, we feel it should be a separate or equal piece. Traffic calming has benefits beyond safety, in reducing emissions, increasing sense of community and more. Further, Saanich has many kilometres of residential and rural roads that need traditional methods of speed reduction and safety may not work, so a traffic calming plan is needed for these streets.

However, we also know that while these big actions will result in a safer, more resilient Saanich in the longer term, there are actions that Saanich can undertake in 2022 to result in increased safety this year. Along with our new Saanich Local Committee, we brainstormed ideas that Saanich can take this year and in 2023 as they work to complete the larger plans.

Change Funding in 2022:

- Setup an 'emergency fund' – for quick response to acute issues such as the recent work on Cedar Hill X Rd
- Setup a community road safety fund – to support citizen-led initiatives that increase road safety (local signage campaigns, road murals, awareness efforts, woonerfs and the like)
- Add at least two dedicated staff in Engineering for traffic calming, following the lead of Victoria. We've seen the benefit these staffers have had in Victoria, implementing small fixes throughout the municipality

Safer roads near Schools:

- Convert all existing School Areas/School Zones to Playgrounds Zones (there are school and playground areas in Saanich, which do not have speed reductions, these should all be replaced with 30km/h reductions and using playground zone durations (dawn to dusk)
- Paint the entrance to the new playground zones with signs and coloured asphalt, following similar work on Royal Oak Drive or Helmcken in View Royal.

Other simple actions:

- Add new marked crosswalks at a faster pace and report out on this work on the ATP annual report card as a percentage of all intersections that have crosswalks.

- Publicly release ticket data from Saanich Police on an annual basis as part of the ATP report card.
- Expand new events for new infrastructure to encourage new users, such as what Capital Bike has run for Victoria on their new bikeways but expanded to include walking and transit.
- Copy successful quick builds like the one-way on Prospect Lake Rd elsewhere in the municipality.

We look forward to helping Saanich with our common goals on road safety, climate action and active transportation over the coming months and years.

Yours,

Corey Burger

Policy & Infrastructure Chair

Capital Bike

address how the infrastructure will be upgraded in the future and outline how stakeholders will be able to provide input into future changes on these corridors.

It is recognized that special or temporary designs or phased approaches and operational changes (turn restrictions) may be needed at key intersections and bus stops along these corridors.

This work has an estimated cost of \$1.7 million

3. Install high visibility school area road markings and improved school zone signage in all school zones that are on Major or Collector Roads in Saanich. This approach was piloted in Saanich at 3 locations in 2021 and has proven to be successful and is ready for deployment across Saanich in 2022.

This work has an estimated cost of \$600k

4. Convert 5km of existing buffered bike lanes to protected cycling lanes by the addition hard infrastructure in the existing bike buffered bike lane spaces. Over the past few years many buffered bike lanes have been constructed in Saanich. These buffered bike lanes have sufficient space to convert them into protected bike lanes.

This work has an estimated cost of \$500k

5. Build walking pathways on the existing pavement on approximately 5km of high pedestrian use residential streets by removing vehicle parking areas. This approach should be used as a trial of a method to provide more pedestrian walkways in Saanich ahead of traditional sidewalk construction. Staff should be responsible for selecting the streets for this trial in consultation with the local community associations and under the following selection criteria:

- a. The street is of sufficient pavement width to leave one side of onstreet parking while providing a single travel lane to be shared by cyclists and vehicles and creating a walking space for pedestrians.
- b. The street has existing or potential for high pedestrian use
- c. The street is on a route used by pedestrians to access parks, schools, rec-centres or other community services

This work has an estimated cost of \$300k

6. Install 8 bike boxes at signalized intersections. Bike boxes at intersections allow for turning cyclists to be given space and priority to position themselves at signalized intersections to complete left hand turns at intersections.

This work has an estimated cost of \$100k

7. Remove pedestrian push buttons at 3 major intersections to provide pedestrian priority at all times of day. A number of intersections in Saanich have had automatic pedestrian walk signal operational during day time hours since early 2020 with limited operational

challenges, in addition to removing the pedestrian push buttons, the use of automatic pedestrian walk signals should be expanded.

This work has an estimated cost of \$15k

8. Upon adoption of the pending Speed Limit Establishment Policy, prioritize implementation of speed limit changes in Rural Saanich to improve the safety of vulnerable road users on streets with limited active transportation infrastructure.

This work has an estimated cost of \$15k

SUMMARY OF COST IMPACTS AND IMPLICATIONS

- The total estimated cost of these works is \$3.33 million.
- Staff in the Engineering department have advised that the costs associated with items 7 & 8 can be accommodated within the existing emerging priorities portion of the Transportation Capital Budget without disrupting the delivery of other planned works.
- To deliver the remaining items, an additional \$3.3 million would be required beyond what is currently included in the draft 2022 budget.

NEXT STEPS

- It is anticipated that the Committee will recommend that Council fund and implement (in 2022) the project(s) above it considers most promising to improve road safety and advance active transportation mode share.
- Any motions passed by the Committee on March 3 will be presented to Council during budget deliberations in March or early April (the budget is finalized in early May).
- To support the recommended projects in 2022, Council will have to either approve the required funding in addition to what is in the 2022 budget, or replace other planned works/expenses 2022 with the recommended project(s).

RECOMMENDATION TEMPLATE

The Active Transportation Advisory Committee had an in-depth conversation on February 24th and March 3rd and in consultation with staff from Engineering are pleased to recommend the following actions to Council for implementation by the end of 2022, in response to Council's request for advice on short term and/or pilot projects to improve road safety and active transportation:

- *Insert the recommended projects/pilots*

Prepared by



Rebecca Mersereau
Councillor & Chair of the Active Transportation Committee