



UPTOWN-DOUGLAS PLAN



December 2021

Saanich



Uptown-Douglas Plan
December 2021

Appendix __ to
OCP Bylaw 8940



TERRITORIAL ACKNOWLEDGEMENT

The District of Saanich is within Coast and Straits Salish territory, the traditional territories of the **Lekwungen peoples** known today as **Songhees** and **Esquimalt** Nations and the **WSÁNEĆ** peoples known today as **WJOLEŁP** (Tsartlip), **BOKEĆEN** (Pauquachin), **STÁUTW** (Tsawout), **WSIKEM** (Tseycum) and **MÁLEXEL** (Malahat) Nations. The First Peoples have been here since time immemorial and their history in this area is long and rich.

ACKNOWLEDGEMENTS

The District of Saanich's Uptown-Douglas Plan is adopted as an appendix to the Official Community Plan. This planning initiative was led by the District of Saanich, in partnership with Frank: planning collaborative.

Key input into the Plan and planning process was provided by the Uptown-Douglas Plan Advisory Committee. Members of the Advisory Committee included:

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1. INTRODUCTION

1.1 BACKGROUND

The Uptown-Douglas (UD) area is located at the core of the Capital Region and is highlighted in Saanich's Official Community Plan as a strategic area for significant growth and change. With several regional destinations, major employment nodes and important cycling, transit and vehicle routes, the area is vital to the success of Saanich and the region (See Map 1.1).

The UD Plan seeks to create a complete community and a shared place for all Saanich residents. Saanich is a community of highly identifiable and valued neighbourhoods, but lacks a singular, shared place for its residents to gather. With its central location, amenities, and transportation routes, the area can become the heart for Saanich and the region. This Plan seeks to provide a roadmap to create such a place - one that has abundant housing and employment opportunities, high quality public spaces and a balanced mobility network.

1.2 PLANNING AREA

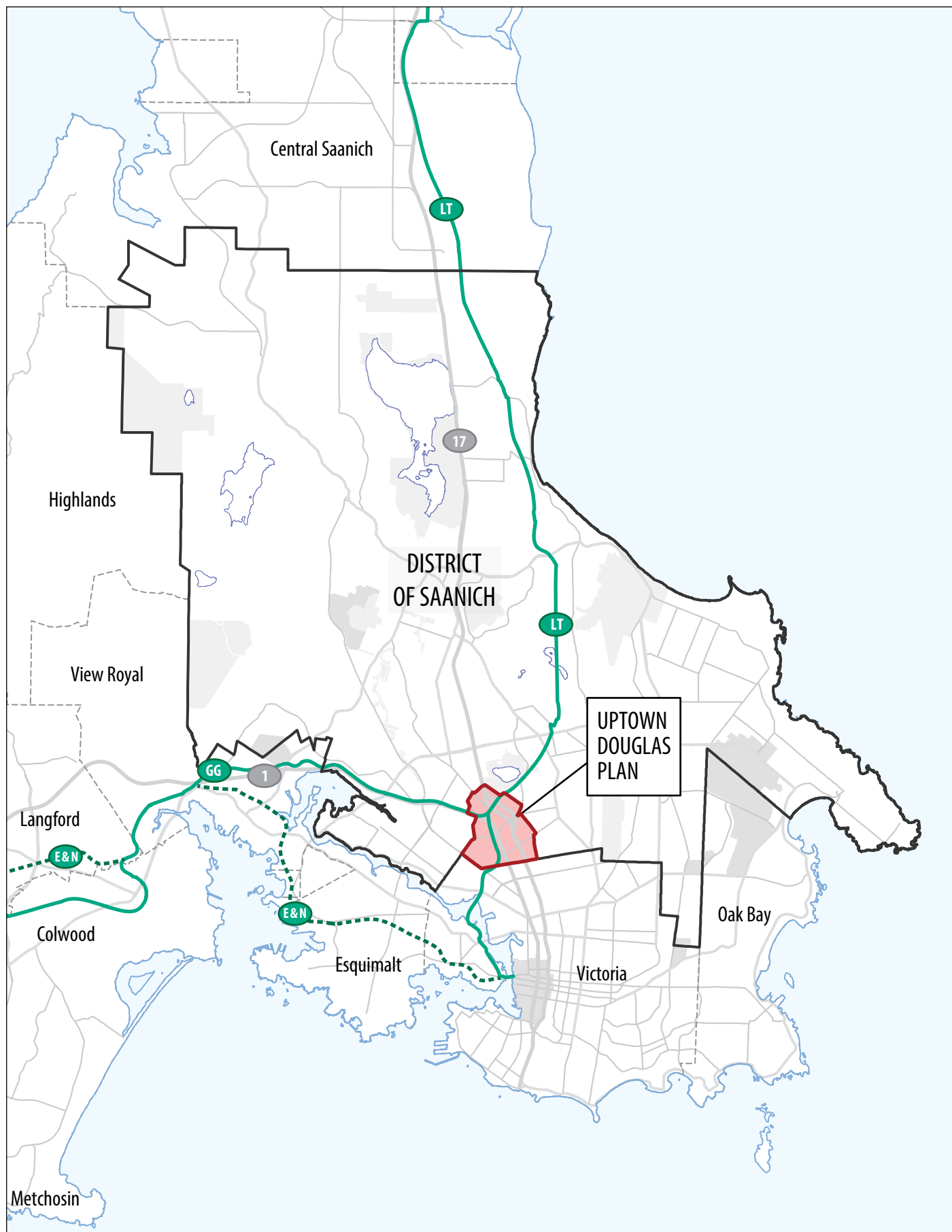
The Uptown-Douglas (UD) planning area is approximately 155 hectares in size and is located at the southern end of the District of Saanich (See Map 1.2). The area is defined by the municipal boundary with the City of Victoria at Tolmie Avenue on the south, Huxley Street, just north of Municipal Hall on the north, between Burnside Road and Harriet Road on the west and Leslie Drive and Calumet Avenue on the east.

The area includes two major regional transportation corridors: Douglas Street, linking the region's urban core to the western communities via the Trans-Canada Highway (Highway 1), and Blanshard Street, providing a connection to the Greater Victoria International Airport and the BC Ferries Swartz Bay Terminal via Patricia Bay Highway (Highway 17).

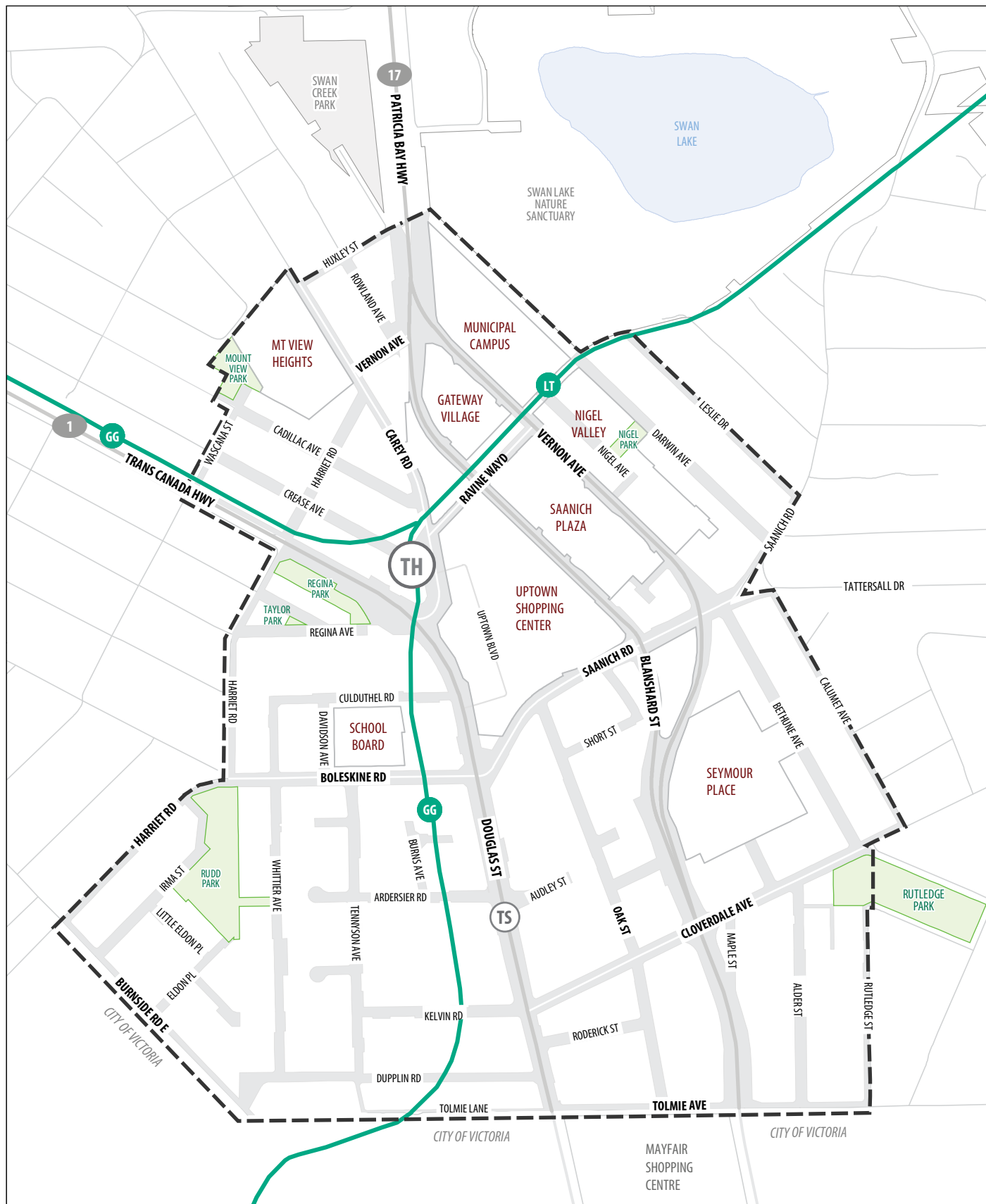
The area also contains important municipal services and valued community amenities. The District of Saanich Municipal Hall, Fire Station, and Police Station (collectively making up the Municipal Campus) are located on the northeast edge of the planning area. The Galloping Goose Regional and Lochside Regional Trails, which run through and connect in the planning area, as well as Swan Lake Nature Sanctuary and Rudd and Rutledge Parks serve residents in Saanich and throughout the region.

The UD Plan provides land use and policy direction for the Uptown Major Centre, which is identified conceptually in Saanich's Official Community Plan (OCP). This Plan provides more definition to the OCP Major Centre boundary and includes detailed policies that implement the broader directions of the OCP. Major Centres are an important planning tool for the District of Saanich and the Capital Regional District that help to focus growth and create complete communities.

The UD area meets a broad range of community and regional commercial and service needs and is well serviced by major bus routes. The UD also provides a range of Multi-Unithousing options, accommodates a range of institutional uses and is home to approximately 30% of Saanich's industrial lands.



Map 1.1 Regional Context



Draft Plan Creation

- The draft Plan was developed by staff over the course of 2018 and early 2019 based on public feedback received and direction provided in the Plan Framework Report.

Reviewing and Refining

- Public engagement and stakeholder feedback was collected on the Draft Plan through a number of channels in Fall 2019 through to early Winter 2020. Three open houses, five pop-ups, a landowner luncheon and area workshop were among the key events where over 750 engagement interactions took place.
- Public feedback received was used to adjust the draft plan prior to being brought forward for Council's consideration in adopting the Plan.



Figure 1.1 UD Plan Development Process

UD Advisory Committee

The UD Plan Advisory Committee, comprised of a variety of community and business stakeholders, has met throughout the process of developing this Plan. Their input and participation

has been integral to ensuring that community issues and interests have been addressed in the creation of the Uptown-Douglas Plan.



Uptown-Douglas Corridor Plan, Community Design Workshop, 2017

1.5 ORGANIZATION OF THE PLAN

The Uptown–Douglas (UD) Plan is organized into 13 sections. Each section builds upon the previous section, providing a comprehensive framework and policy direction for realizing a 20-30 year vision for the UD area.

This Plan is arranged to provide the vision, values and higher level concepts in the opening sections, with the finer grained details and guiding policy in subsequent sections. In each of the core content sections (4-11), objectives provide a touchstone for evaluating potential decisions and highlighting intended outcomes. Policies that follow identify specific courses of action to be undertaken to realize this Plan's vision. A summary of each section is provided below.

Section 1 - Introduction provides an overview of the UD area, outlines the purpose of this Plan and describes the process through which it was developed.

Section 2 - Planning Context highlights the relevant context and unique characteristics of the UD area, including planning area statistics, history and opportunities and challenges, all of which inform the policies in each of the eight core content sections that follow.

Section 3 - Vision, Values and Goals details the shared vision, community values and overarching goals that frame and guide the objectives and policies throughout this Plan. Key plan directions are highlighted for the UD area as a whole, and unique vision statements are provided for each of the seven neighbourhood sub-areas.

Section 4 - Environment and Sustainability provides objectives and policies supporting climate change adaptation and mitigation, energy reduction and management of stormwater, natural areas and urban forests.

Section 5 - Land Use details a framework for land development and change in the UD area, including objectives, policies, land use designations and community contribution

priorities. Comprehensive land use direction is provided for each of the seven neighbourhood sub-areas.

Section 6 - Transportation and Mobility provides objectives and policies to guide transportation and mobility changes in the area – a crucial element to reaching the UD Plan.

Section 7 - Significant Streets provides objectives and policies in the UD to enable the transition of significant streets to complete streets. Future mobility and urban design concepts are provided for eight critical streets in the UD area.

Section 8 - Parks, Open Spaces, Trails and Community Facilities provides objectives and policies to guide new park, open space and trail design and outlines directions to secure future park and facility needs, with specific attention given to the Galloping Goose Regional and Lochside Regional Trail networks.

Section 9 - Urban Design provides objectives and policies for urban design in the UD area, including the built form and public realm.

Section 10 - Social and Cultural Well-Being provides objectives and policies to develop and balance critical community foundations, including social well-being, inclusion and the enhancement of arts and culture.

Section 11 - Economic Vibrancy provides objectives and policies to enable retention and growth of various economic sectors, building on the strong economic assets existing in the UD area.

Section 12 - Taking Action and Tracking Progress identifies priority implementation actions and a series of indicators to evaluate the progress of the Plan.

Section 13 - Appendices provides supporting information and documentation for this Plan, including a glossary and a list of amendments.



Uptown–Douglas Corridor Plan, Community Design Workshop, 2017

1.6 HOW TO USE THIS PLAN

The Uptown-Douglas (UD) Plan is intended to be used by Council, community members, staff and the development community to guide and evaluate potential change. This guidance extends to the evaluation of development applications, capital projects and community initiatives and programs. The Uptown-Douglas Plan is adopted as part of the Official Community Plan Bylaw. The Official Community Plan is made up of the General Plan (sometimes referred to as the "OCP"), Local Area Plans, and Development Permit Area guidelines. The General Plan, Local Area Plans, and Development Permit Area guidelines are used to guide and support land use decisions.

The document itself is intended to guide and inform evaluation of development applications but it must not be considered independently of other municipal and regional planning documents. General OCP policies establish the broad vision and framework for growth and change in the community. Plans that focus on smaller geographic areas, such as the UD Plan and Local Area Plans, provide more detailed guidance at the local level, aligning with and building on the broad directions of the OCP.

The Local Area Plan will provide a greater amount of certainty on future change, but circumstances will arise that were not anticipated or where competing objectives need to balance. In these situations, the vision and objectives of the General Plan and LAP will provide principled guidance and insight into how the situation would be addressed and trade-offs balanced to achieve the best outcome.



Uptown-Douglas Corridor Plan, Community Design Workshop, 2017

Planning Framework

- While the General Plan, LAPs and Development Permit Area design guidelines are referred to as distinct parts of the Official Community Plan Bylaw, they form an integrated whole and act as a comprehensive planning framework
- The General Plan provides broad guidance on potential land use change, with the LAP providing more detailed direction at the local level within the Official Community Plan framework



Looking Northwest - Douglas Street / Highway 1

- Development Permit Area design guidelines have a narrower focus (typically form and character of development) and, as guidelines, inherently have a greater level of flexibility in their application
- Council may, at any time, support proposals that do not align with specific General Plan or LAP provisions. In instances where a proposal gives rise to a direct conflict with the provisions of an LAP or the General Plan, a statutory process with public input would be required to amend the LAP and/or General Plan.

The Uptown-Douglas Plan, as with all Local Area Plans, should be considered alongside other plans, policies, and regulations that may have an impact on each area. These include the Active Transportation Plan, BC Transit plans, Housing Strategy, Climate Plan, Urban Forestry Strategy, Regional Housing Affordability Strategy, and the Parks, Recreation and Culture Master Plan. Initiatives that arise with the other governments, including First Nations, will also need to be considered.

The UD Plan area overlaps with three Local Areas. In instances where there is direct conflict between this Plan and an existing Local Area Plan, the more recent plan will take precedence. A comprehensive range of subject matter is included in the UD Plan that will be used to guide decisions that require the contemplation of many different factors. In fact, the integrative analysis of multiple factors is essential to ensuring community priorities are addressed in a thoughtful and holistic manner. For example, the design of a new park space would draw on multiple sections of this Plan to ensure it is supported by land use and urban design, well connected to transportation networks and major streets, contributes to natural systems and ecological performance and is part of an overall framework to support social wellbeing and economic development.

Applying the Uptown-Douglas Plan to Development Proposals

The sections of Uptown-Douglas (UD) Plan work together to provide guidance for how sites can be developed in a manner consistent with the Plan's vision, values and objectives.

Section 4 – Environment and Sustainability highlights key foundational site considerations based on the area's natural characteristics and ensures consideration of impacts associated with climate change mitigation and adaptation.

Section 5 – Land Use provides parameters for the type of development that would be supported on each site and key considerations to ensure land use objectives are achieved.

This includes:

- Suitable land uses;
- Maximum building height;
- Desired building setbacks and street wall heights;
- Area-specific land use considerations; and
- Guidance for community contributions that will enhance the area and support the introduction of new residents, employees and visitors.

Section 6 – Transportation and Mobility identifies:

- Long term mobility network design;
- Locations of future roads and pedestrian/cycling connections, which may impact site design; and
- Guidance for off-street parking supply and design.

Section 7 – Significant Streets provides detailed design guidance for properties that front onto important roads in the UD area. Direction is provided for:

- Conceptual design and associated required frontage improvements;
- Potential road dedications;
- Street specific street wall and setback dimensions; and
- Guidance for the treatment of the public-private street interface.

Section 8 – Parks, Open Space, Trails and Community Facilities identifies:

- Park and open space provision requirements; and
- Guidance for design of open spaces, parks and trail interfaces.

Section 9 – Urban Design includes an extensive set of design principles that provide direction on:

- Site planning considerations;
- Building design and massing; and
- Design of public spaces and the public realm.

Section 10 – Social and Cultural Well-being highlights key social infrastructure considerations, including:

- Guidance for the provision of supportive and affordable housing;
- Desirable locations for public art; and
- Direction on the preservation and revitalization of heritage resources.

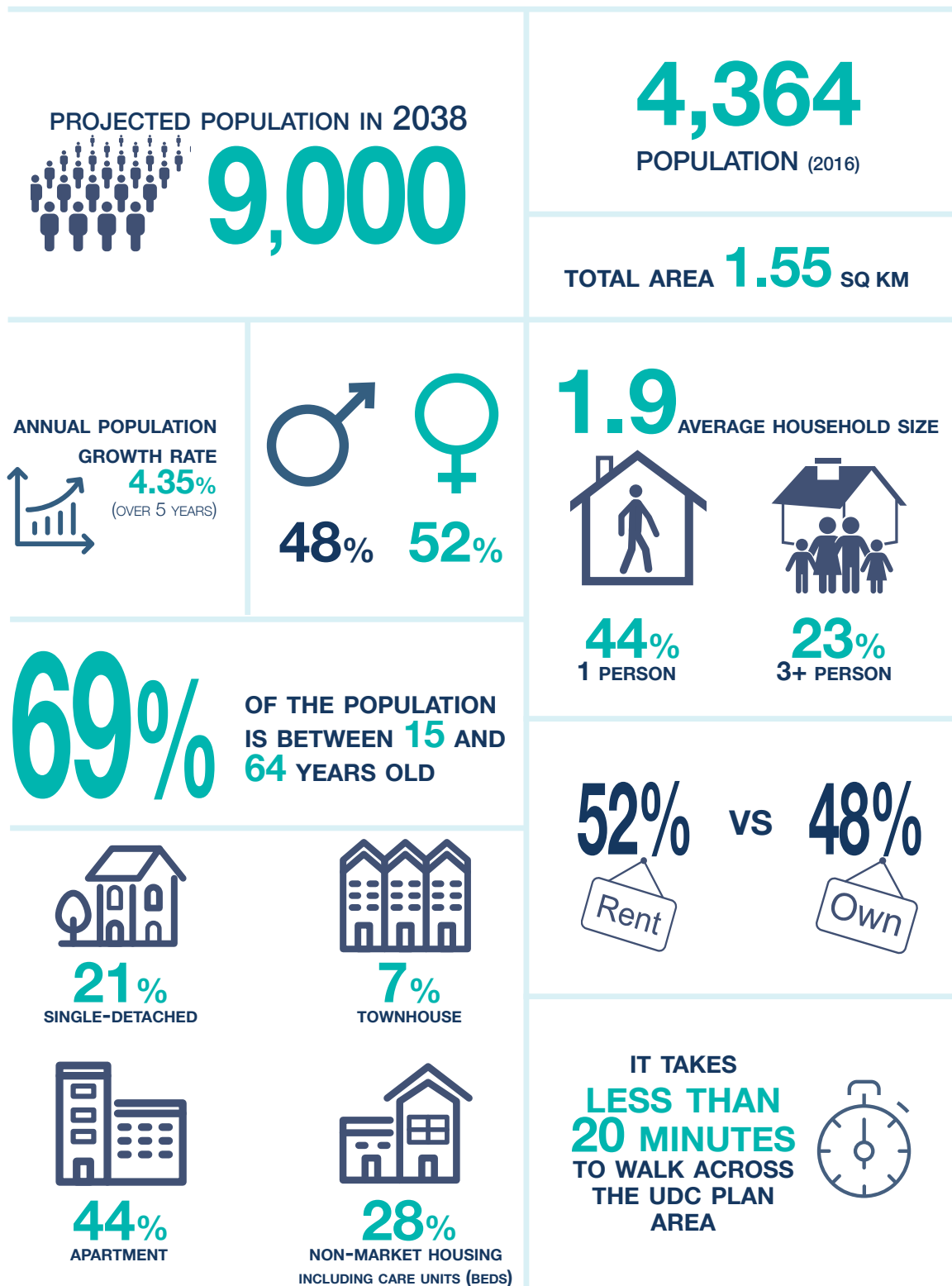
Section 11 – Economic Vibrancy identifies considerations regarding the types of economic uses and activities that are supported in the UD.

2. PLANNING CONTEXT

2.1 NEIGHBOURHOOD CHARACTERISTICS

The current neighbourhood characteristics for the Uptown-Douglas (UD) area are presented in a snapshot below. Seeking to highlight key aspects and current conditions that exist within

the study area, this data has informed policy development throughout the UD Plan.





43%

OF THE UDC AREA
CONSISTS OF
COMMERCIAL ZONING

(MANY OF THESE ZONES
ALSO PERMIT
RESIDENTIAL USES)

EMPLOYMENT
IN THE UDC
HAS **GROWN**
APPROXIMATELY
2X FASTER
THAN THE
SAANICH
AVERAGE
OVER THE LAST
5 YEARS



10,111 PEOPLE
ARE EMPLOYED
IN THE UDC
AREA (2018)

MEDIAN
HOUSEHOLD
INCOME

\$50,700
IN UDC VS
\$77,282 IN
SAANICH



2,400

CYCLISTS AND PEDESTRIANS
USE THE GALLOPING GOOSE
AND LOCHSIDE TRAILS DAILY
IN THE SUMMER



7 PUBLIC CHARGING
STATIONS



APPROXIMATELY **30,000**
PASSENGERS TAKE TRANSIT
THROUGH THE UDC AREA DAILY



THERE IS A TOTAL OF
2,299 METRES OF
TRAILS THROUGHOUT
THE UDC AREA



24% OF ALL COMMUTES
IN SAANICH ARE
MADE BY WALKING, CYCLING
AND TRANSIT



7 NEIGHBOURHOOD PARKS
WITHIN THE UDC AREA



6% OF TOTAL
UDC AREA
CONSISTS OF
PUBLIC PARKS

INFORMATION PRIMARILY OBTAINED FROM 2016 CENSUS

2.2 HISTORY

The local history of southern Vancouver Island is abundant - some of which is readily documented and detectable on the landscape and some is more narrowly noted and under-acknowledged. It is imperative to recognize that since time immemorial the Coast Salish peoples have lived off of these lands. The Uptown-Douglas (UD) area is situated upon the traditional territory of the Lekwungen peoples, known today as the Songhees Nation and Esquimalt Nation, whose historic and cultural relationships with the land continue to this day.

Beginning in the 1840s, the Hudson's Bay Company (HBC) was appointed authority to establish a colony on Vancouver Island, on the condition that they promote settlement in the region. In the immediate years of colonization the lands and way of life for the Songhees peoples were disrupted momentarily. During this time many Songhees relocated to the Inner Harbour Reserve, leaving their traditional village, located in Victoria's downtown core, behind. And, with the introduction of the rules of British land tenure, the vast and culturally rich territorial lands of the Songhees and Esquimalt peoples was disregarded and the built form of Saanich as we see it today slowly began to take form.

A prominent early European land owner in the Saanich area was Dr. William Fraser Tolmie, who held an 1,100 acre land holding known as Cloverdale Farm. Professionally, Tolmie practiced medicine, was a Hudson's Bay Company factor, a cabinet minister with the provincial legislature, a recognized authority on Indian Affairs, a botanist and an ethnologist. Together with his wife Jane, he built the Cloverdale House (1859-60), a 15-bedroom structure made of stone and California Redwood, located on the corner of Cloverdale Avenue and Calumet Avenue. Regrettably, the house was demolished 102 years later in 1963 despite local petition to preserve the structure and its historic significance in the community. Subdivision of Tolmie's land began in the 1890s and assisted the influx of growth and land ownership in Saanich. By the time Saanich was incorporated as a District Municipality in 1906, the street pattern in the UD area was fairly well established south of Cloverdale Avenue, with the area having been divided into smaller 0.4 hectare (1 acre) parcels.



Wm. J. Chave & Son Grocers, Douglas St. and Boleskine Rd., 1915.

Saanich, and particularly the commercial node within the boundaries of the UD area, experienced a successive land boom in the period between 1910 and 1914 and the area was again extensively subdivided into familiar land patterns seen there today. Some of the earliest public buildings to serve the population were constructed on land donated by the Tolmie family, including the original Tolmie School (Tolmie Avenue) and the adjacent St. Mark's Church (Tennyson Avenue), both of which were constructed in 1892. The original school was later demolished and a new school was mandated and constructed between 1912 and 1914. The building now accommodates the offices of the Greater Victoria School Board.



Fire Hall No. 1, 1939.



Douglas Street seen from Regina Park, 1965.



Lum Greenhouses, seen from Douglas Street, 1960s.

The later part of the century brought three key landmarks to the area that helped shape the UD area into the recognizable urban destination that we identify with today. The first landmark was the development of the Town and Country strip mall on the current Uptown Shopping Centre site in 1961. Next, in 1965, the District of Saanich's commissioned municipal hall was completed, a brutalist style architectural building, now listed as a designated heritage building. And lastly, the Capital Regional District invested heavily into the recreational capacity of the area with the development of the Galloping Goose Regional Regional Trail (1987), the Switch-Bridge (1996) and the Lochside Regional Trail (2001).



Bridge crossing over CNR Rail (now Lochside Regional Trail), 1971.



Town and Country mall, circa 1960s.



Town and Country with Woolco, opening day 1961.

Accommodating change is an ongoing requisite of growth and development, and this is readily evident in the core area of UD. By 2010 the Town and Country strip mall, which had served the region as a popular shopping outlet for nearly 50 years, closed and Uptown Shopping Centre, an open air shopping district offering substantial mixed-use buildings, a central plaza and a core boulevard, became the new shopping and service destination for local and regional residents.

Looking back, the turn of the 20th century Saanich offers evidence of similar and essential features that the UD Plan is striving to create today – a compact and livable urban core complete with varied amenities, established services and well-organized transportation. Recognizing lessons learned from its past, the UD Plan proposes thoughtful and innovative design of future developments while respecting and enhancing the economic, social, cultural and environmental resources of the community.



Saanich Core, 1974.



Aerial view of Municipal Hall on Vernon Avenue and surrounding area, 1965.

Saanich Municipal Hall, 1965.



2.3 OPPORTUNITIES AND CHALLENGES

The Uptown–Douglas (UD) area is a critical regional hub with an unprecedented opportunity to implement innovative approaches that reflect the Official Community Plan's vision of sustainability. The area is well positioned to receive growth and develop as a compact, complete community. A number of current challenges and opportunities have the potential to heavily influence how the area develops and ultimately influence the success of this Plan.

Accommodating Future Growth

This UD area is expected to undergo significant change over the next 20 years in order to accommodate the addition of approximately 4,300 new residents and 5,000 new employees. Many new services, buildings and public spaces will be required to successfully integrate this dynamic growth in a way that provides a high quality of life for people who live, work, shop or recreate in the area. A well designed mix of housing, employment opportunities, shopping, local gathering places, and community facilities make neighbourhoods more walkable, livable and sustainable. Through this thoughtful planning and design, there is an opportunity to accommodate growth and create a community that will enable people of different life stages, income levels and abilities to grow and age in place.

Creating a Sense of Place

The UD area is a major regional destination. However, the area's role as a transportation nexus presents challenges, as the majority of the area has been designed to accommodate free movements of automobiles. This design has impacted the character of public spaces and ability to move comfortably within the area. Complete street designs, the development and celebration of unique districts, public art and a better connected mobility network will all help to create a clear and memorable identity for the area.



Uptown Shopping Centre Splash Pad



Lochside Regional Trail, near Blanshard Street overpass

Leveraging Transit Improvements

The area is a vital component of the region's transit system and its role is anticipated to be further enhanced as infrastructure is expanded. The development of a multi-modal transit hub and the progression of Douglas Street towards a rapid transit corridor will be significant catalysts for change in the area. Ensuring this change is supported by land use that improves, not worsens, the pedestrian and cycling experience and the quality of the public realm will be a primary challenge.

Redesigning the Street Network

The street network in UD area is characterized by long blocks and the presence of major roads designed to highway standards. Creating a vibrant urban area will be largely dependent on facilitating improvements to the street network that place a greater emphasis on pedestrian activity and a high quality public realm. Building complete streets, adding green infrastructure, breaking up superblocks, incorporating pedestrian and cycling crossings and creating a high quality public realm will be crucial to success. In particular, effectively liaising with the Ministry of Transportation and Infrastructure on the design of Douglas Street, Blanshard Street, Vernon Avenue and Ravine Way to arrive at a design that is consistent with a walkable urban context will be critical.

Maintaining and Enhancing Employment Uses

The UD area contains approximately 10,000 jobs and is a vital component of Saanich's economy. Supporting existing business operations and further diversifying employment opportunities will be critical to the area's success. As the area accommodates additional high density residential, a critical challenge will be to ensure compatibility between existing employment uses and new residential uses. Additionally, maintaining the integrity of primary industrial/commercial areas and capitalizing on new and emerging sectors will be essential.

Housing Diversity and Affordability

Housing diversity and affordability is a significant challenge throughout the region and continues to be an important issue and priority for Saanich. Saanich is committed to supporting policies and actions that increase housing diversity as well as affordability. Living and working close to transit in a pedestrian-friendly, amenity rich environment reduces the need for an automobile, which in turn frees up overall household income. The UD area has great potential to accommodate a diversity of new housing options that could meet the needs of many segments of the population in a location with abundant mobility options and access to services.

Access to Parks, Trails, Open Space and Community Facilities

As Saanich seeks to enhance the livability and vitality of UD, parks, trails, open spaces and community facilities will need to be substantially strengthened in order to meet the recreational and transportation needs of those living, working and visiting the area. As the area evolves, the need to provide both new and redeveloped parks, trails and open spaces will be essential to build complete and integrated neighbourhoods that meet the needs of residents and employees, create a sense of community and foster social interaction.

Enhancing Area Sustainability

From an ecological perspective, UD is highly compromised, with few permeable surfaces, minimal green space and limited exposure to nature and natural processes. However, the area contains a high density of jobs and housing and excellent mobility options, providing strong building blocks for a sustainable, complete community. While large scale ecological restoration of the area will be highly challenging, it is essential that each development or capital project incrementally works towards enhancing sustainability. This effort could be accomplished in many ways including through additions to the urban forest, enhanced stormwater management and enhanced building energy performance to make the UD area an exemplar for urban sustainability. Saanich Municipal Hall in particular provides an opportunity to model sustainable development practices.



Rudd Park

2.4 PLANNING FRAMEWORK

The Uptown–Douglas (UD) Plan provides a vision for how the area will develop over the next 20-30 years using detailed policy direction and an implementation strategy. This Plan, however, is not standalone – it is informed by, dependent upon and related to several other plans, policies and tools managed and implemented by the District of Saanich and its partners.

The UD Plan works together with several related policy documents to aid Council, staff, property owners and the public in decision-making and to provide a measure of predictability to the community and development industry. A summary of some of the related policy documents is provided here, but many more may be applicable over the course of implementing the UD Plan.

Official Community Plan (2008)

All planning in the District is guided by the Sustainable Saanich Official Community Plan (OCP), 2008. The OCP articulates the fundamental values and goals of the community and is the principal legislative tool for guiding future growth and change throughout Saanich. The OCP policies and principles guided the development of the UD Plan. The UD Plan is an appendix to the OCP and is intended to build on and provide more detailed policy guidance for the UD planning area.

Regional Growth Strategy (2018)

The Regional Growth Strategy (RGS) is a vision for the future of the Capital Regional District, guiding decisions on regional issues such as transportation, population growth and settlement patterns. The RGS promotes the long term livability of the region by enhancing social, economic and environmental performance.

2020 Climate Plan: 100% Renewable and Resilient Saanich

The Climate Plan lays out a pathway to achieve a 100% renewable and resilient Saanich. At the same time it identifies actions to help us adapt to a changing climate. There are three key plan goals:

- Cut GHG emissions in half by 2030 & net zero by 2050
- Transition to 100% renewable energy by 2050
- Prepare for a changing climate

Our collective success requires action from everybody, including residents, businesses, community organizations, institutions, neighbouring local governments, and senior levels of government. As the UD area is a key hub within Saanich, envisioned to see substantial increases in density and considerable change, delivering the climate goals and targets through the Plan's policies will be critical.

Active Transportation Plan (2018)

The Active Transportation Plan guides the development, promotion and implementation of safe, attractive and convenient active transportation choices in Saanich. The plan includes cycling and pedestrian networks, along with implementation plans and priorities. Several strategies and actions guide staff's efforts and provide direction for shifting a significant portion of trips from vehicles to active modes such as walking, cycling and transit.

BC Transit – Victoria Transit Future Plan (2011)

The Transit Future Plan envisions the Victoria region's transit network over 25 years and describes what services, infrastructure and investments are needed to get there. The plan was designed to create a stronger link between land use and transit planning, support the key initiatives of BC Transit's Strategic Plan and support the Provincial Transit Plan. Transit infrastructure upgrades for the UD planning area include bus priority lanes along Douglas Street that are currently under construction, as well as a multi-modal transit hub that will be located north of the switchbridge on Crease Avenue where the Galloping Goose Regional and Lochside Regional Trails connect.

Saanich Housing Needs Report (2020)

Saanich has developed a Housing Needs Report (2020) which was completed as part of a joint Housing Needs Report project for 11 CRD communities. The Report provides detailed information on housing conditions in the municipality while providing a regional context, and looks to the needs across the housing continuum, now and into the future. Key areas of local needs identified include affordable housing, housing for seniors, rental housing, housing for families, housing for peoples with disabilities, and homelessness.

Saanich Housing Strategy (2021)

The recently adopted Saanich Housing Strategy provides direction on how the municipality plans to contribute to improving housing opportunities and outcomes and address the urgent need to increase housing supply, diversity, and affordability. The Housing Strategy builds on existing Saanich and Regional plans, policies, and initiatives and looks to support and expand the work done to date. The Saanich Housing Needs Report, which will be updated on a regular basis, highlights key community housing issues and projected demand and served as a vital tool to inform the development of the Housing Strategy.

Regional Housing Affordability Strategy (2018)

The Regional Housing Affordability Strategy (RHAS) includes a series of goals and objectives as well as strategies to be considered by the Capital Regional District (CRD) and its potential partners, including municipalities, electoral areas, other levels of government and the non-profit, co-op housing and private sectors. The RHAS sets out a plan of action for meeting the priorities of the updated Regional Growth Strategies, specifically the development of policies and strategies designed to promote and support a range of housing types and tenures to meet the needs of individuals of all ages, incomes and abilities, and the initiatives to improve housing affordability including the provision of a mix of housing types (market and non-market).

Other Regional and Municipal Plans

Other regional and municipal plans, including the Capital Regional District's Regional Pedestrian and Cycling Master Plan and Regional Trails Management Plan, and the District of Saanich Urban Forestry Strategy inform, influence, and help to implement the Vision of the UD Plan.

Overlapping Local Area Plans

The UD Plan area overlaps with four Local Area Plan (Map 2.1). In situations where there is direct conflict between this Plan and an existing Local Area Plan, the more recent plan will take precedence.

- **Saanich Core Local Area Plan (1999)**

The Saanich Core local area is located in the southern part of the municipality and is adjacent to the common boundary with the City of Victoria. Approximately 75% of the Saanich Core area is located within the UD planning area. The areas of the Saanich Core that lay outside the UD boundary are to the east and include multi-unit residential areas adjacent to Rutledge Park.

- **Tillicum Local Area Plan (2000)**

The Tillicum local area is defined by the Trans-Canada Highway to the north, Portage Inlet to the west, Gorge Waterway to the south and Harriet Road, Whittier Avenue and Burnside Road to the east. A small portion overlaps with the UD planning area, including areas along Burnside Road. Tillicum is a suburban community with a mix of commercial, recreational and institutional facilities, however residential uses dominate the area.

- **Carey Local Area Plan (1999)**

A small portion of the Carey local area overlaps with the northern portion of the UD planning area. This area is adjacent to the transit hub and includes the Campus of Care, located along Carey Road. The UD Plan works to transition from the high density civic core to the established neighbourhoods of the Carey local area.

- **Quadra Local Area Plan (2001)**

The Quadra local area plan boundary overlaps with a one block section of the UD Plan, including the north side of Darwin Avenue and the south side of Leslie Drive between Saanich Road and Swan Lake Nature Sanctuary and directly north of the Nigel Valley master planned site. This area foresees a transition from single detached residential to a mix of low-density residential infill, including townhomes and rowhomes, up to 2.5 storeys, and an apartment/stacked townhomes, up to 4 storeys, located at the corner of Saanich Road.

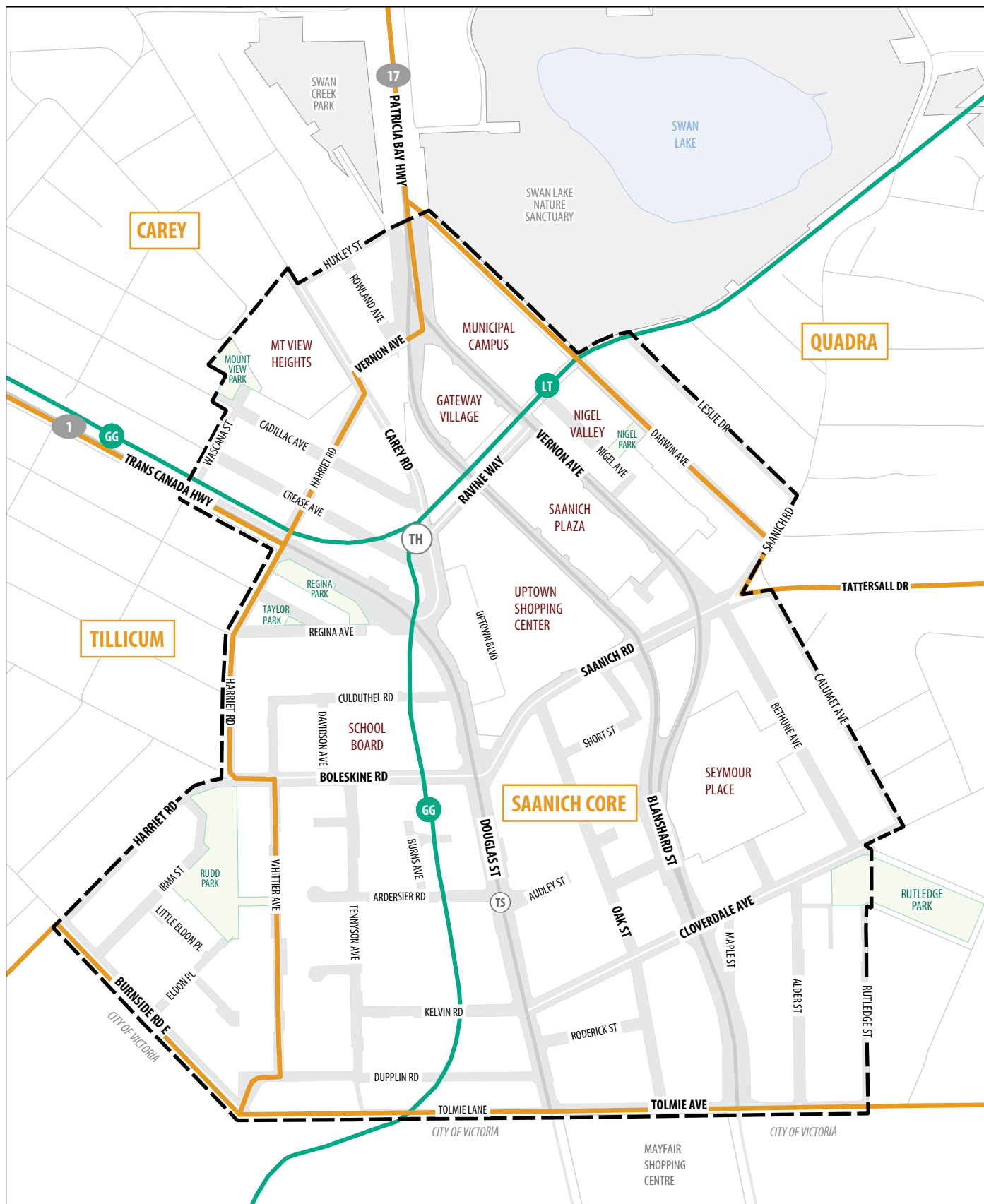
Other Authorities and Approvals

As part of the Plan implementation, and as part of anticipated rezoning and development applications, other approving authorities, including but not limited to, the Ministry of Transportation and Infrastructure, the Capital Regional District, the Songhees Nation, the Esquimalt Nation, and the City of Victoria, may be included in an application's circulation, review and in some cases collaboration through a development's process.

In some scenarios, projects will require approval from these agencies and authorities, and in other scenarios there may be dialogue and collaboration required to ensure success of the proposed project. In some situations this involvement may be ongoing through to a project's completion.

Where a referral and/or approval is required, the District of Saanich will coordinate this communication as part of the application review process(es).

For example, the redevelopment of properties adjacent to the Galloping Goose Regional Trail will involve a referral of the application to both the Ministry of Transportation and Infrastructure and the Capital Regional District. As the Regional Trail is owned by the Province and administered by the CRD, in situations where access is proposed, approval/authorization from these agencies will be required.



LEGEND

UPTOWN DOUGLAS PLAN

LOCAL AREAS

UPTOWN PARKS

2.5 PLANNING TOOLS

The implementation of the Uptown–Douglas (UD) Plan will occur primarily through land redevelopment initiated by private developers and capital projects and programs initiated by Saanich or other public agencies. From a land redevelopment perspective, planning tools are available to local governments to help manage and evaluate change.

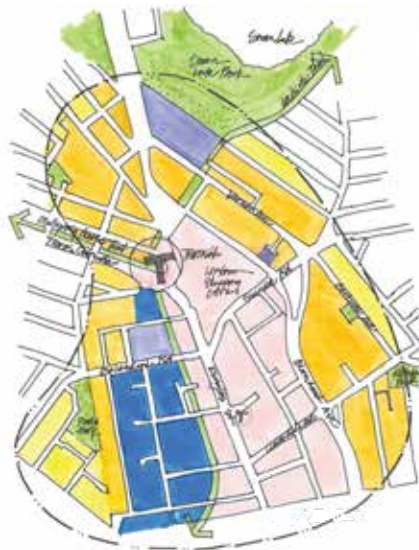
Every parcel in Saanich is designated with a specific zone, which identifies parameters for development, including use, density, setbacks and height. The UD Plan looks 20-30 years into the future and provides a vision for land use. This vision is communicated through the policies throughout this Plan and through the specific land use and building height designations provided for guidance in Section 5. As this Plan is intended to guide future change, there are instances where existing current zoning does not align with UD policy and land use and building height designations. Potential changes to zoning are evaluated through a formal process, with the UD Plan used to guide decision-making.

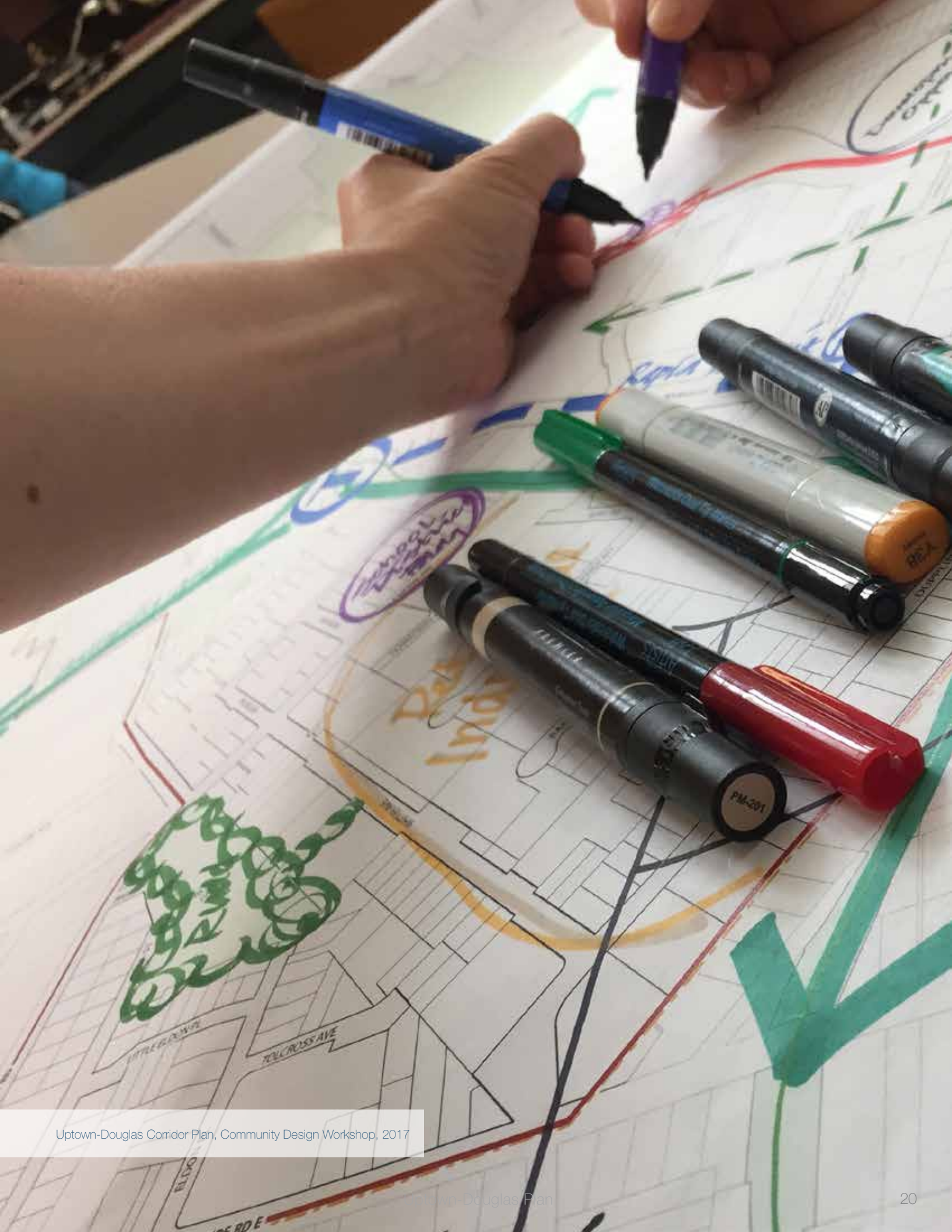
For the purposes of this Plan, two primary review processes will be undertaken for any redevelopment proposal: a Development Permit Application and/or a Rezoning Application. Both processes are explained in detail below:

If a development application is received and the proposal conforms to the site's current zoning, the applicant would be required, in most instances, to obtain a Development Permit. A Development Permit helps to ensure the building and site design meets design objectives. A Development Permit, when required, mostly regulates the form and character of commercial, industrial, and multi-unit developments through guidelines tailored to address the local context. Development Permit applications are reviewed by staff, with opportunities for community input before they are considered by Saanich Council for approval.

If an application is received and it does not conform to the site's current zoning, the potential zoning change (rezoning) is evaluated using established policy. In the case of applications in the UD area, this Plan will be the primary policy tool used to evaluate a potential change to zoning. The broader directions of the Official Community Plan and policies in other Saanich and region-wide plans will also be used to evaluate the application.

For each rezoning application, the community is consulted and staff conduct an analysis to assess impacts, benefits and alignment with policy. Planning staff then present a report to Saanich Council with a recommendation. The rezoning application receives a public hearing and is then considered by Council for approval. In the case of a rezoning, Council can broadly consider all components of a project and often must consider conflicting goals and objectives.





Uptown-Douglas Corridor Plan, Community Design Workshop, 2017

3. VISION, VALUES AND GOALS

3.1 VISION

The 20-30 year Vision for the Uptown-Douglas (UD) area reflects the goals and objectives of the Official Community Plan as well as the desired future of the area as articulated by participants through an extensive public engagement process (See Section 1.4).

The Vision for the UD Plan is as follows:



Uptown-Douglas Corridor Plan, Community Design Workshop, 2017

3.3 KEY PLAN DIRECTIONS

Creating a new heart for the District of Saanich, a central gathering place for the community and region, is no simple task. While the Uptown-Douglas area has numerous assets and unique potential to realize this vision, major changes to the physical landscape and shifts in the way that growth and development are approached will be required. The following items highlight fundamental changes that are central to achieving this Plan's Vision and will be delivered by the collective policies in this Plan.

1. Put Pedestrians First

Develop an exceptional pedestrian realm and safe, people-friendly connections; enhance east-west movement; and continue to build on the success of the Galloping Goose and Lochside Regional Trails.

Capital Developments and Metropia, the Bloor & Dufferin project.



2. Create a Landmark Multi-modal Transit Hub

Develop a regional multi-modal hub that creates convenient connections for all travel modes, elevates transit service, encourages redevelopment, highlights regional cycling connections and reinforces the pedestrian first priority.

Kengo Kuma & Associates, Paris Metro project.





3. Accelerate Rapid Transit Implementation on Douglas Street

Support more compact, transit-oriented development in coordination with the multi-modal transit hub that increases housing and business options, enhances the pedestrian realm and accelerates the evolution to rapid transit on Douglas Street.

City of Hamilton, LRT project.



4. Create a Network of Active and Engaging Public Places

Develop a series of parks, plaza spaces, parkettes, and streetscape redesigns highlighted by a central civic space.

Taller 13 Architectura, Mexico City Airport.



5. Lead Growth with Residential

Encourage growth with a mix of compact, multi-unit residential housing including affordable non-market and market rental, and family-appropriate options that are suitable to broad range of residents.

Douglas Developments & Shalom Baranes, 655 New York Avenue.

6. Conserve Light Industrial

Retain the light industrial uses west of the Galloping Goose Regional Trail and enhance the area with street improvements and mixed-uses where appropriate to retain and expand local employment opportunities.

Chard Developments, Vancouver, BC.



7. Transform Oak Street

Design Oak Street as a central high street integrating transit, bikes, a pedestrian promenade and amenities, along with active and engaging mixed-use buildings framing the street.

Geoff Livingston, Larimer Square at Night.



8. Convert Grey to Green

Develop the area as a showcase of urban sustainability through enhancing nature and integrating green infrastructure and green buildings.

Delancey Street Associates,
Essex Crossing project, New York City.

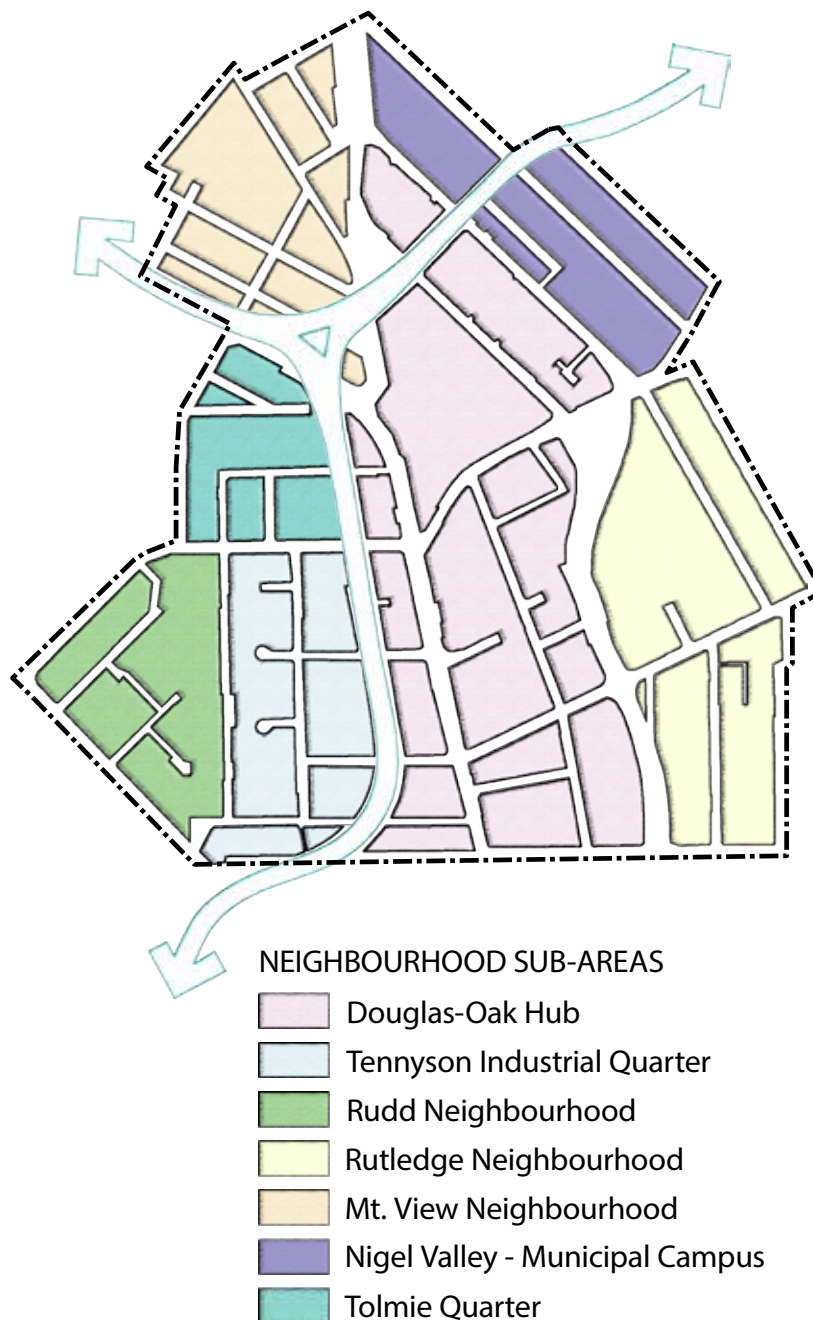


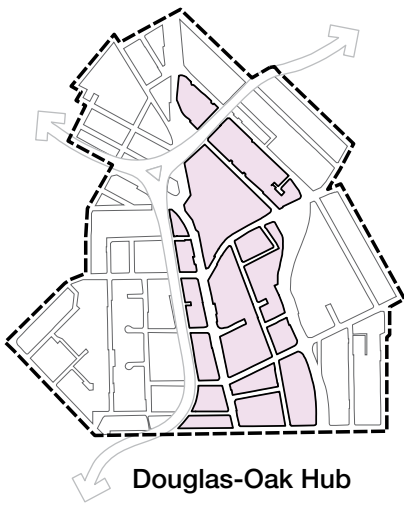


3.4 NEIGHBOURHOOD SUB-AREAS

The Uptown–Douglas (UD) area contains a diverse mix of neighbourhoods that have unique characteristics and roles. This Plan provides direction to create a high functioning, cohesive core for Saanich by drawing on the distinct social, economic and environmental contributions of the UD neighbourhood sub-areas.

This section of the Plan provides an overview of each of the seven neighbourhood sub-areas (identified on Map 3.1) and highlights the vision for how these areas will grow and change into the future. Section 5 (Land Use) provides specific policy guidance regarding how changes will occur to enable the sub-area visions to be realized.





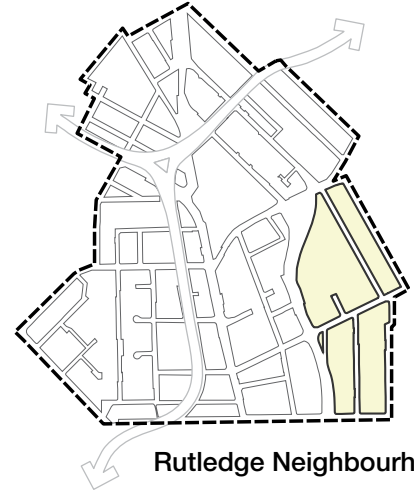
Douglas-Oak Hub



Tennyson Industrial Quarter



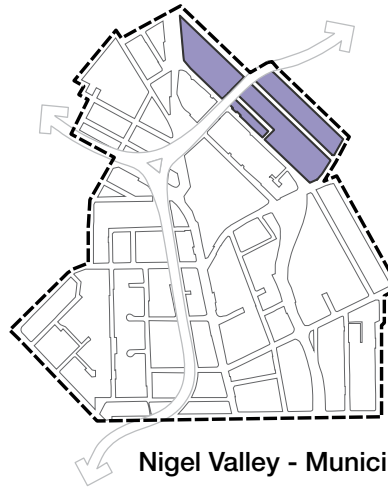
Rudd Neighbourhood



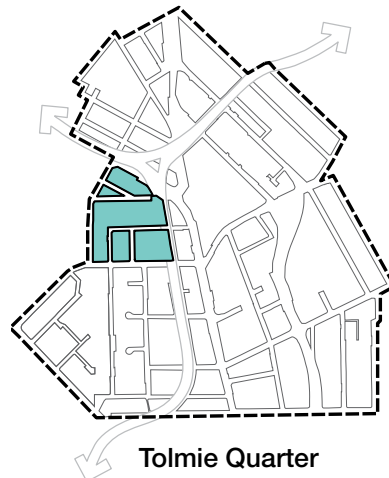
Rutledge Neighbourhood



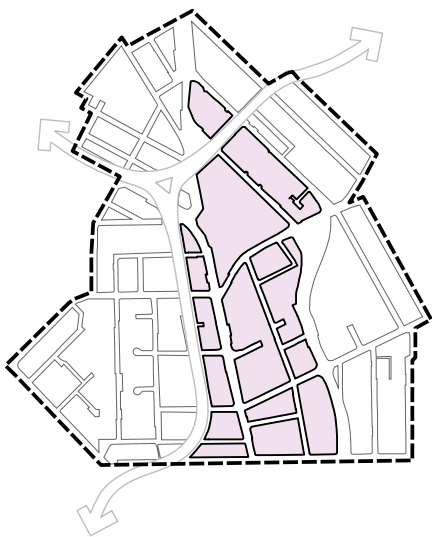
Mt. View Neighbourhood



Nigel Valley - Municipal Campus



Tolmie Quarter



DOUGLAS-OAK HUB

Existing Conditions

The Douglas-Oak Hub is a significant employment and shopping area in Saanich and is anchored by the Uptown Shopping Centre, a major regional destination. Commercial uses, which include retail, office and hotel, provide employment and economic activity, as well as goods and services for residents. Major highways transition into urban arterial streets as they enter the area and serve as a gateway to the region's urban core. The pockets of residential that have developed in recent years are the first steps towards increasing residential population in the area.

The highways also bring a high volume of vehicle traffic, which has had a significant influence on the land use and design of the area. The pedestrian realm is characterized by narrow sidewalks, unseparated from vehicle traffic in many locations, little to no green space and large blocks with limited connectivity.

Vision

Over time, the Douglas-Oak Hub will have blossomed into the heart of Saanich. A sustained period of redevelopment will result in the revitalization of the area, with new parks, attractive streetscapes and a plethora of shops, services and employment opportunities providing a high quality of urban living. Douglas Street will be reinforced as a key employment corridor, while a re-imagined Oak Street will be framed by new ground-oriented multi-unit buildings and more pedestrian-oriented commercial including retail, restaurants, cafes and entertainment.

The area will be better connected with key destinations accessible within minutes by foot or bike, including, the Uptown Shopping Centre, Mayfair Mall, the re-developed Saanich Plaza, new park spaces, the Galloping Goose Regional Trail and new high frequency transit connections. The whole precinct will be active and engaging - full of life with diverse vertical mixed-uses and bustling businesses, conveniently located near transit and the Galloping Goose Regional Trail.



TENNYSON INDUSTRIAL QUARTER

Existing Conditions

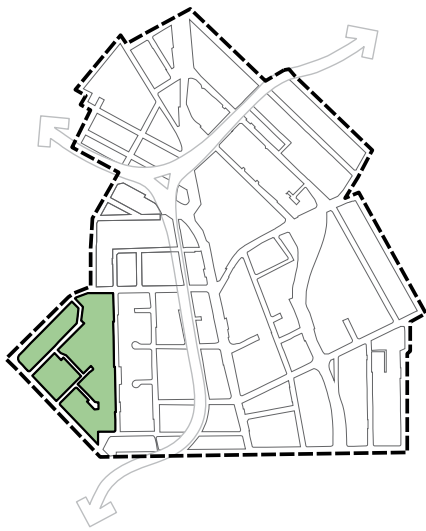
The Tennyson Industrial Quarter contains mostly light industrial uses and is an important component of the industrial land supply in Saanich and the region. The range of industrial and commercial uses in the area is diverse, with an increasingly greater number of uses that could be considered more commercial in nature. A similar industrial clustering is located to the south of this area in the City of Victoria.

Mobility in the area is characterized by one north-south through street (Tennyson) and numerous fractured or dead-end east-west streets. The pedestrian realm is limited with few street trees, sidewalks or crossings, except for the eastern portion of the area that is framed by the Galloping Goose Regional Trail.

Vision

The Tennyson Industrial Quarter will contain a diverse range of employment opportunities and remain a regionally significant industrial area. New higher density mixed use industrial/residential buildings fronting the Galloping Goose Regional Trail will bring new life and inject an eclectic feel to the area. A mix of industrial, commercial and residential developments adjacent to the Trail will provide a blend of employment and housing activity that will bring vibrancy to the existing light industrial in the area.

The Galloping Goose Regional Trail will continue to be celebrated as a regional jewel and will be enhanced and animated by food kiosks, pocket parks and a wider, separated area for pedestrians and cyclists to travel. Tennyson Avenue will remain the heart of the industrial area, and sites along the street will continue to grow and redevelop, providing new business space and additional employment opportunities on upper floors. New sidewalks and street trees will create more desirable active travel options, while roads and vehicle access for goods movement supporting the industrial uses will remain a priority.



RUDD NEIGHBOURHOOD

Existing Conditions

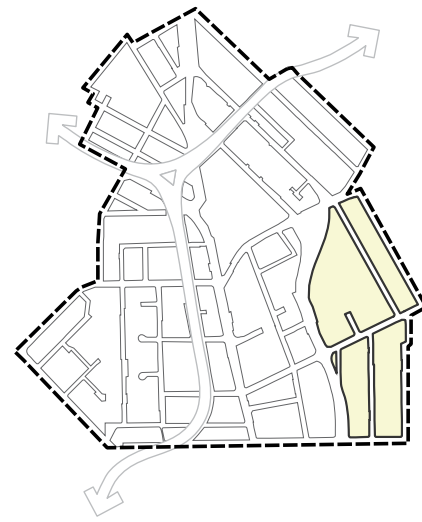
The Rudd Neighbourhood is a predominantly single family neighbourhood built on a modified grid. Over the years, multi-unit developments have located on the edges of the neighbourhood, including along Burnside Road and Whittier Avenue. Rudd Park is one of the largest parks located in the UD area and hosts amenities for various users.

This area contains a number of heritage registered or designated buildings, with most in close proximity to Boleskine Avenue and Harriet Road.

Vision

The Rudd Neighbourhood will maintain its residential neighbourhood character and be further enhanced by additional housing options near Rudd Park in the form of low-rise apartments and townhouses suitable for families with children. Rudd Park will be revitalized and expanded to serve as a vibrant community gathering place.

The area will become better connected for all modes of travel over time and Whittier Avenue will serve a preferred walking and cycling route with a greenway character.



RUTLEDGE NEIGHBOURHOOD

Existing Conditions

The Rutledge Neighbourhood contains a mix of land uses, including commercial along Blanshard Street and Cloverdale Avenue, multi-unit residential in close proximity to Rutledge Park and single family on its eastern edge. Rutledge Park is the largest greenspace in the area and is a central component of neighbourhood life. Properties adjacent to Rutledge Park have seen a recent revitalization, with new apartments buildings constructed that provide housing for a variety of demographics.

Blanshard Street, with its uninviting streetscape and substantial width, poses a significant barrier to travel with areas to the west. A large portion of this neighbourhood is occupied by the Provincial offices at 4000 Seymour Place, which is a major employer and includes a daycare facility and a large amount of surface parking.

Vision

The Rutledge Park neighbourhood will be a location of choice for residents seeking to live close to shops, services and amenities in a quiet residential setting with a range of medium density housing options. Rutledge Park will continue to serve as a neighbourhood anchor and community recreational hub. The neighbourhood will be further enhanced by the addition of new housing options and improved networks for walking and cycling.

The large parcels along Blanshard Street will redevelop over time to a higher density mix of uses including residential, neighbourhood commercial, small retail and employment. This redevelopment will include 4000 Seymour Place, which is envisioned to have a series of buildings with smaller footprints, a finer grained mobility network, new green space and improved streetscapes that connect the site with the surrounding neighbourhood. A newly designed Cloverdale Avenue will further emphasize the convenience of an eight to ten minute walk or two minute bike ride between the Galloping Goose Regional Trail and Rutledge Park.



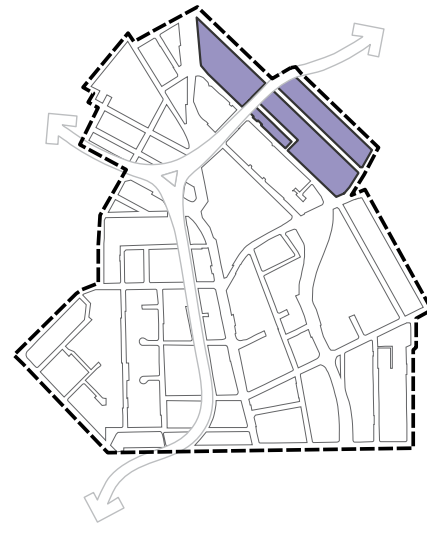
MT. VIEW NEIGHBOURHOOD

Existing Conditions

The Mt. View Neighbourhood hosts the intersection of the Galloping Goose Regional and Lochside Regional Trails and is the gateway to the heart of Saanich. The Trans-Canada Highway and Patricia Bay Highway transition to Douglas Street and Vernon Avenue / Blanshard Street respectively, providing a transition into a more urban context. Between these two corridors is a neighbourhood which contains a mix of housing and employment uses, including the recently developed Campus of Care. The Mt. View neighbourhood offers several viewpoints and a walkable street network with easy access to transit, services and regional trails.

Vision

The Mt. View neighbourhood will be a highly desirable community where a variety of housing options, access to shops and recreational opportunities, and an abundance of convenient mobility options mean residents can easily live without owning a vehicle. New multi-unit residential developments will provide housing for families, younger adults, and seniors. The sub-area will be the nexus of transit activity for the region, providing efficient connections to anywhere in Greater Victoria. The transit hub will be easily accessible for all modes of travel and, through thoughtful design and integration with the neighbourhood, will serve as a central gathering space for the community.



NIGEL VALLEY – MUNICIPAL CAMPUS

Existing Conditions

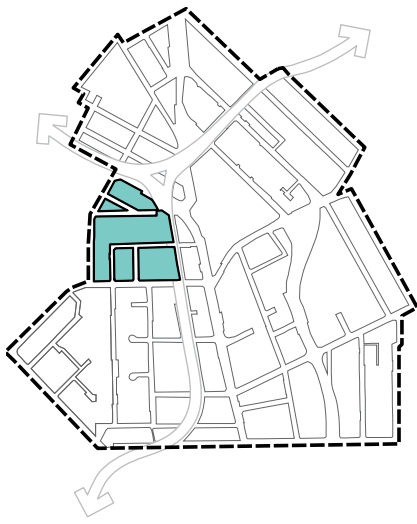
Sitting astride two of the Capital Region's major arteries for commuters and visitors this neighbourhood is the site of the District of Saanich Municipal Hall, Police Station and Fire Station (collectively the Municipal Campus). The Municipal Hall, which has played an important role in Saanich for over 50 years, is the key facility of the Campus which, due to its heritage status, low seismic capacity and operations pressures, will warrant a high degree of careful planning in the coming years. Nigel Valley is also located here and includes a collection of five housing and care operators that serve a variety of populations, including seniors, families and individuals with disabilities.

Single family residences are located along Darwin Avenue and Leslie Drive, with multi-unit residences closer to Saanich Road. The Lochside Regional Trail runs through the area to create a strong east-west connection and links to the Galloping Goose Regional Trail. Swan Lake Nature Sanctuary is located on the northeast edge of the neighbourhood and is adjacent to the Municipal Campus parking lot.

Vision

The Municipal Campus and Nigel Valley sites will be redeveloped to integrate strong pedestrian connections and public realm elements to be enjoyed by the greater community. The Municipal Campus, highlighted by the iconic landmark Municipal Hall, will be revitalized as a space for discourse, celebration, remembrance, governance and community. The Nigel Valley will be a vibrant, inclusive neighbourhood that is fully integrated with the broader community and provides a range of housing and services that contribute to a healthy community.

Additional housing options will be provided north of Darwin Avenue through a mix of low density multi-unit residential buildings that sensitively transition into the neighbourhood. The Lochside Trail will be enhanced through the addition of improved connections, trail access and plaza space.



TOLMIE QUARTER

Existing Conditions

The Tolmie Quarter currently contains a mix of institutional, industrial, office and multi-unit and single detached dwelling residential uses. The area is distinguished by the Greater Victoria School Board (School Board) offices and adjacency to the Galloping Goose Regional Trail. The Tolmie Quarter's proximity to the future transit hub, Galloping Goose Regional and a diversity of land uses presents unique opportunities for the future of the area. The pedestrian realm is limited in much of the area, with infrequent street trees, single-side sidewalks and minimal public open space, namely Regina and Taylor Parks.

Vision

The Tolmie Quarter will transition into a centre of community activity. The core of the centre will be defined by the redevelopment of the School Board site, seen as a hub for learning and community service uses that may include a public school in combination with other desirable community facilities. To ensure the maximum potential of the site is achieved, a master plan will ensure a vibrant arrangement of uses that includes the preservation of the heritage structure and its integrity on the landscape. New connections and building orientations will encourage on-site activity and animate the Galloping Goose Regional trail.

Other properties will evolve in concert with the School Board site and provide an eclectic mix of employment and housing options. Tech, ultra-light industrial and mixed employment uses will contribute to the area's character and support positive public realm improvements. Housing will be added to the area through multi-unit housing to the west and north included on the upper storeys of light industrial/mixed-employment sites.

The area will be well connected, with seamless transitions to multi-modal transit hub, the Galloping Goose Regional trail, and streets animated with store fronts, live-work units and inviting community spaces. Active transportation options, people-oriented public spaces and unique work and work-live opportunities will fuel the success of the area.

4. ENVIRONMENT AND SUSTAINABILITY

Protecting and enhancing the environment and its functions for current and future generations is foundational to the success of the Uptown–Douglas (UD) area. Addressing the issues of climate change, resource depletion and energy security is particularly critical for the health of future generations. This Plan seeks to transition the UD area from an auto-centric area with limited ecological assets towards a greener, people-friendly environment where residents live, work and play.

Saanich has declared a Climate Emergency and has committed to the following targets:

- Cut community greenhouse gas emissions in half by 2030 and to net zero by 2050;
- Transition to a 100% renewable energy by 2050; and
- Prepare for a changing climate

Saanich's Climate Plan identifies key actions within six theme areas to achieve these targets. These include mobility, buildings and infrastructure, food and materials, ecosystems, community wellbeing and leadership in District operations.

The UD Plan supports Saanich's climate goals by encouraging more compact transit oriented development, diverse housing options and maintaining less energy intensive housing forms that use renewable heating and cooling systems, providing better walking, cycling and public transit options, facilitating access to nature and integrating low-carbon solutions and environmental considerations into urban design.

Healthy and functioning natural systems are critical to the well-being of communities, both locally and globally. Watercourses, the urban forest and other natural areas are vital to the community's livability, health and environmental sustainability. They support biodiversity, cleaner air and water, animal and plant habitat and contribute to climate change resiliency. They provide opportunities for residents to connect with nature, enjoy a quiet moment, and better understand natural processes and systems.

The UD planning area is part of the Cecelia Creek Watershed and the Colquitz Creek Watershed. Development has occurred over part of the area where Cecelia Creek once flowed, resulting in the loss of what might have once been a significant natural feature in the area. Cecelia Creek is now one of the most degraded creek systems in the region. Most of the pollution in Cecelia Creek comes from urban runoff from impervious surfaces. Due to industrialization and development, there are few remaining natural features in the area. This Plan aims to restore and enhance these features for a healthy and productive environment.

All sections of this Plan work towards developing the fundamental components of a sustainable community, while this chapter specifically focuses on climate and energy resiliency in the built environment and the management of natural resources and systems in the UD area.

OBJECTIVES

- A. Support Saanich's climate targets by significantly reducing building and transportation greenhouse gas emissions and supporting the shift to 100% renewable energy.
- B. Encourage sustainable urban design by promoting conservation and resiliency through green buildings, energy efficiency, renewable energy and green infrastructure.
- C. Increase resilience and capacity to adapt to climate change impacts in both natural and human systems.
- D. Create ecological value and improve storm water management through new green infrastructure and the enhancement of existing open spaces.
- E. Enhance and restore environmental assets including Cecelia Creek and its Watershed.
- F. Improve urban forest health and significantly expand the tree canopy cover throughout the area.
- G. Increase the permeability of surfaces throughout the UD area.



Boulevard, Uptown Shopping Centre

4.1 URBAN FOREST

The urban forest is the sum total of all trees and their associated ecosystems and includes trees on private lands, parks, boulevards and other public spaces. The urban forest is a critical component of a functional green infrastructure system and sequesters carbon, filters air and water, reduces energy demands, creates shade and contributes to social well-being.

Mapping completed in 2016 showed that the majority of the Uptown–Douglas (UD) area has less than 5% tree cover. The conservation organization, American Forests, suggests that a healthy canopy cover in a central business district (i.e. a downtown) should be 15%, with the number going up to 25% canopy cover for healthy urban residential areas. Using these measures, the UD area falls short of optimal tree cover.

The Saanich Urban Forest Strategy sets a goal of no net loss of tree canopy for the District. Meeting this goal will be a challenge, as a shift to higher density building forms and an increased demand for utility corridors, wider sidewalks, bike lanes and vehicle turn lanes within road rights-of-way will impact space available to plant trees. However, opportunities will also be presented through future redevelopment, where sites dominated by surface parking can be redeveloped to better integrate the urban forest and rights-of-way can be expanded to accommodate appropriate planting area for trees to thrive. Policies within this Plan seek to achieve 20% canopy in the UD Plan area, much of which will be realized through redevelopment over time.

POLICIES

- 4.1.1 Implement the Urban Forest Strategy by retaining, planting and maintaining sizeable trees along boulevards, on municipal properties, in riparian areas, in parks and on private lands as a means to expand the urban forest, establish street tree canopies, and support climate change mitigation and adaptation.
- 4.1.2 Target 20% tree canopy coverage in the UD Plan area.
- 4.1.3 Consider climate change in management decisions for the urban forest, including species selection, street-scale species diversity, innovative planting solutions, and the allocation of resources for maintenance such as watering or hazard tree removal.
- 4.1.4 Cluster tree and shrub plantings at major intersections (or other focal points) to create a visual respite in areas of extensive pavement and provide opportunities for significant understorey planting.
- 4.1.5 Require adequate soil volumes in boulevard tree plantings, including through a minimum 2 metre wide planting area and through the use of engineered soil cells (See also Section 7: Significant Streets and Policy 8.2.10).
- 4.1.6 As part of street design, identify line assignments for trees to facilitate early planting in advance of road dedication.
- 4.1.7 Through periodic updates to the Tree Protection Bylaw and environmental regulations, promote the protection of the urban forest.
- 4.1.8 Pursue opportunities to introduce native tree and shrub species, including Garry Oaks, on boulevards and public space plantings, where appropriate.

4.2 CLIMATE CHANGE MITIGATION: BUILT ENVIRONMENT AND ENERGY

The Uptown–Douglas (UD) Plan aims to reduce building and transportation related energy use and greenhouse gas emissions by: focusing future residential and employment growth in nodes and corridors served by rapid and frequent transit service; improving active transportation amenities, bus service and access to electric vehicle charging; and increasing housing options that enable residents to live close to services and employment opportunities. Additionally, the higher density mixed-use and multi-unit building forms identified in this Plan typically result in significantly lower per capita energy consumption due to shared walls and building energy systems. This combination of locational efficiency and denser building forms will provide more employment and residential opportunities in highly energy efficient areas.

Building energy performance in Saanich is guided by the BC Energy Step Code, which establishes progressive performance steps in energy efficiency for new buildings. The Step Code will progress from the current BC Building Code level to net zero energy ready buildings by 2032. Saanich has adopted the Step Code, with building energy performance requirements increased over time. In order to meet our local and global greenhouse gas emissions targets and as part of building a sustainable community and

building stock that is resilient to a changing climate, new development will need to meet, or exceed, the mandated Step Code level of that time and utilize renewable energy (e.g. electricity, which is currently 97% renewable) for space and hot water heating systems.

An opportunity to support both climate mitigation and adaptation is the development of district energy systems, which can facilitate a flexible and efficient supply of local renewable energy sources, and support energy security through decentralized energy generation and stable energy prices. For district energy systems to be feasible and function effectively, a concentration of medium to high density development is required to create adequate demand. In a 2012 District Energy Review Study, the UD area was identified as the area of Saanich that could best support a future district energy system. To advance the potential implementation of district energy, this Plan looks to proactively put the supporting pieces in place by increasing density along major roads and in proximity to the transit hub and by encouraging systems in buildings that can be easily connected to a future small or larger scale district energy system.

POLICIES

- 4.2.1 Encourage new developments to minimize energy consumption through opportunities such as passive solar design and highly energy efficient building envelopes and ventilation systems.
- 4.2.2 For new and existing developments, encourage the use of renewable energy for space and hot water heating systems such as air-source heat pumps, geexchange, solar thermal, photovoltaic, biomass, hydro-electric and wind.
- 4.2.3 Explore incentive tools and programs to encourage development of net zero carbon and/or net zero energy ready buildings.
- 4.2.4 Encourage the provision of charging facilities for electric vehicles and electric bikes beyond required levels (See also Policy 6.5.8.)
- 4.2.5 Undertake a feasibility study to assess the potential of developing a district energy system designed for renewable fuels in the UD area.
- 4.2.6 Support climate change adaptation by encouraging integration of green roofs that can improve stormwater runoff, building energy performance, habitat opportunities, urban cooling outcomes, and access to green space.
- 4.2.7 Optimize the potential viability of and future connection to a future district energy system in the UD area by encouraging the use of hydronic heating systems in new medium and high density developments.
- 4.2.8 Encourage adaptive reuse of existing buildings or where that is not feasible, the deconstruction and re-use of building materials.

4.3 CLIMATE CHANGE ADAPTATION: BUILT ENVIRONMENT AND RESILIENCE

Climate projections for our region predict an increase in extreme weather events that include flooding, drought, temperature extremes and high wind events. The Uptown–Douglas (UD) area is highly impermeable with few green spaces, resulting in an increased risk of overland flooding, higher urban heat island effect and lower air quality.

To create greater resiliency, new developments and redevelopments will need to consider this future context and create site designs that enhance stormwater management, reduce the urban heat island effect and grow the urban forest.

POLICIES

- 4.3.1 Support redevelopments that introduce green space, green roofs, living walls expand the urban forest and provide weather protection and shading.
- 4.3.2 Collaborate with the Capital Regional District regarding a potential study to comprehensively assess the Saanich portion of the Cecelia Creek Watershed, assess the feasibility of daylighting Cecelia Creek, and identify priority actions for possible improvements.
- 4.3.2 Design landscaping to adapt to increasingly variable climate, including through the use of native and drought-tolerant species.
- 4.3.3 Design developments to provide adequate cooling and ventilation to account for extreme heat and poor air quality events, and take advantage of passive cooling, where possible.
- 4.3.4 Support the installation of efficient irrigation systems that enable the establishment of a viable urban forest and enable plantings to withstand extended periods of low precipitation and high temperatures.
- 4.3.5 Encourage the use of flat roofs for amenity space, renewable energy generation and green roofs.



4.4 WATERSHEDS AND STORMWATER MANAGEMENT

Rainfall and stormwater runoff are the main sources of water in a watershed. The quality and quantity of stormwater runoff is affected by alterations to the land, including agriculture, urban development, and the activities of people within the watershed. The Uptown-Douglas Plan is located in both the Cloverdale Creek sub-watershed of Cecelia Creek

and the Colquitz Creek Watershed (Map 4.1). This means activities in the UD affect ecological assets like Swan Lake, the Gorge Waterway and Colquitz Creek. This Plan guides development practices in the area to mitigate negative impacts on local watersheds.

POLICIES

- 4.4.1 Support the Capital Regional District in undertaking an intermunicipal study to comprehensively assess the Cecelia Creek Watershed, assess feasibility of daylighting Cecelia Creek and identify priority actions for improvements.
- 4.4.2 Further to Policy 4.4.1, and as informed through the feasibility study, consider options to enhance Cecelia Creek through redevelopment of properties and public spaces along the historical alignment, including through improving riparian areas in the above ground section adjacent to the Switchbridge.
- 4.4.3 Through redevelopment seek to acquire additional dedication to secure the historical alignment for future enhancements to Cecelia Creek.
- 4.4.4 Increase awareness of Cecelia Creek and the Cecelia Creek watershed, including through interpretive signage and public art.
- 4.4.5 Maximize permeability, minimize stormwater runoff and improve stormwater quality through low impact development practices such as bioswales, engineered raingardens, green roofs and permeable pavement.
- 4.4.6 Encourage rainwater harvesting for re-use of non-potable water for landscape irrigation.
- 4.4.7 Limit discharge into municipal stormwater drainage systems and place focus on on-site stormwater management.
- 4.4.7 Explore options to enhance the stormwater management practices of existing industrial and commercial businesses to enhance the quality and quantity of stormwater entering the receiving environment.



4.5 NATURAL AREAS AND GREEN SPACES

The Uptown–Douglas (UD) area has limited natural features, with the most notable natural asset being Swan Lake on the periphery of the Plan area. Most of the UD area has been developed and industrialized reducing natural areas, however, future developments can create opportunities to

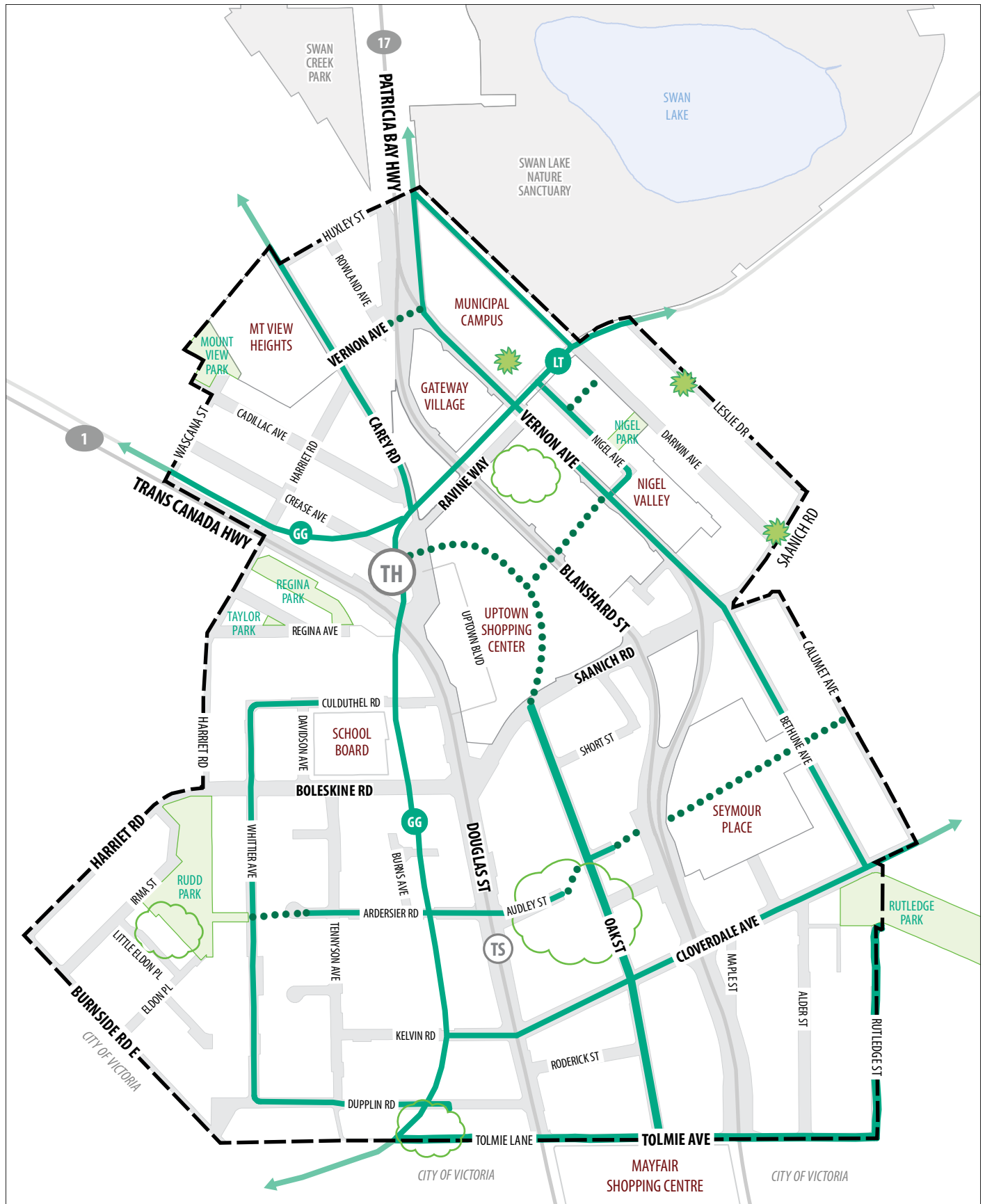
increase public and private green space and natural areas. Existing open space assets, such as the Galloping Goose Regional and Lochside Regional Trails and the historic Garry Oak ecosystem, can be enhanced and built upon to create green corridors and enhance environmental performance.

POLICIES

- 4.5.1 Where development is adjacent to natural areas or green spaces, seek opportunities to increase the size of the green space, expand the overall connectivity of the green space network and ensure ecologically sensitive areas are not negatively impacted.
- 4.5.2 Enhance the transition between Swan Lake and the Municipal Campus through creating a buffer zone and considering opportunities for low impact development practices on the municipal campus site, such as permeable pavement or rain gardens.
- 4.5.3 Explore opportunities to enhance the ecological value of Regina Park with respect to stormwater management, biodiversity and pollutant mitigation (See also Policy 8.2.8).
- 4.5.4 Create and enhance green corridors as identified in Map 4.2 through:
 - Planting additional trees within the right-of-way;
 - Adding pocket parks with trees and appropriate vegetation, including along the Galloping Goose Regional and Lochside Regional Trails (See Section 8.3);
 - Integrating stormwater management features; and
 - Reducing impervious surfaces within the right-of-way and on adjacent properties.
- 4.5.5 Prepare an inventory of supported planting materials for development projects that reflect the principles of Naturescape and includes drought-resistant and native species.
- 4.5.6 Consider climate change in decisions around the protection and management of natural areas and biodiversity.
- 4.5.7 Pursue protection, restoration and connection of remnant natural areas such as Garry Oak clusters, rock outcrops, and low-lying shrub vegetation to enhance habitat and resilience.
- 4.5.8 Look for opportunities to expand the dominant heritage landscape of the UD area by planting Garry Oaks and associated vegetation where possible.
- 4.5.9 Adding pocket parks throughout the UD area, including working with the CRD to achieve this along the Galloping Goose and Lochside Regional Trails.



Mount View Heights



Map 4.2 Natural Areas and Green Corridors

5. LAND USE

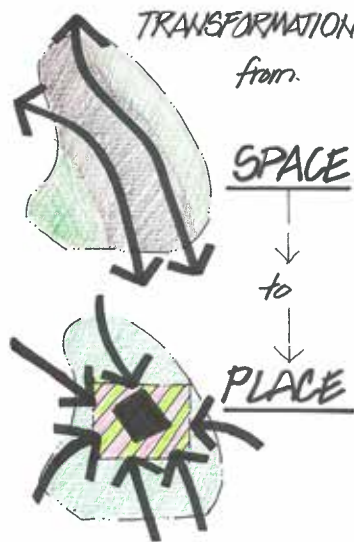
With a broad diversity of land uses and a significant level of intensity, the Uptown–Douglas (UD) area is perhaps the most dynamic area in Saanich. With this intensity of activity comes an abundance of jobs, services, and housing options available to residents and employees, tourists and commuters. Supplying close to 10,000 employment opportunities, a diversity of shops and services, regionally significant transportation corridors, and a developing housing stock, the UD area is a true regional hub. Over time, this level of activity is anticipated to further intensify and diversify.

The UD area contains numerous essential retail, public and institutional services: Saanich Municipal Hall, Fire Station and Police Station (collectively making up the Municipal Campus), the regional school board offices, a regional library and four shopping centres (Uptown Shopping Centre, Saanich Plaza, Island Home Centre, and Gateway Village). The diversity of the area is evident in the business community with over 600 individual employers.

The number of residents in the UD area is growing at a more rapid rate than the rest of Saanich and it is anticipated that the population will double as new residential and mixed-use developments continue to be added. Today, a predominance of single-person households is a trend in most urban centres across Canada. The UD area reflects this trend with a high proportion of single-person households, renters and multi-unit dwellings. Adding a variety of housing choices to UD area will enable households of all sizes to live in a vibrant urban centre and will contribute to the overall health of the economy by bringing a diverse workforce close to growing employment opportunities.

Historically, the automobile informed land use patterns in the UD area, prioritizing ease of access for vehicles. The evolution of the region and associated transportation networks has resulted in a dramatically different planning context for the area than previously existed. More recently, the abundance of development in the Westshore, expansion of high frequency transit lines, establishment of the Galloping Goose Regional and Lochside Regional Trails, and the redevelopment of the Uptown Shopping Centre have supported the positioning of UD as the heart of the region. These developments have also set the stage for significant land use change that can accommodate additional dwellings and jobs in a location already rich with services and transportation options.

As the area grows and densifies, an inviting built environment and range of public amenities will be needed to ensure the UD area develops as a complete community. Overall, this is a transition that seeks to transform the UD area from a 'space' to a 'place', where the area becomes mutually experienced, meaningful and memorable.



This section details the land use pattern that is intended to evolve in UD over the next 20-30 years. As such, this chapter is a keystone element of this Plan and is foundational to most objectives and policies found in other chapters. The land use framework is supplemented by a series of policies that will help to ensure land use changes contribute to the 'space' to 'place' transformation, including infrastructure and community amenities needed to ensure the development of a complete and sustainable community. Detailed policies are included for each of the seven sub-areas within the UD area, reflecting their unique conditions and objectives.

OBJECTIVES

- A. Lead growth with residential that provides a diversity of housing, including affordable options and a variety of building typologies.
- B. Retain the area's role as a primary employment centre.
- C. Transition height and density downwards from the area core to established neighbourhoods.
- D. Support the implementation of rapid transit by concentrating residents and jobs within walking distance of Douglas Street.
- E. Ensure all new buildings are pedestrian-oriented and enhance streetscape conditions.
- F. Improve area connectivity and break up long blocks.
- G. Ensure each neighbourhood sub-area has sufficient public open space to provide recreation opportunities within walking distance.
- H. Support the introduction of new development that enhances the mix of uses and supports area vibrancy.

5.1 FUTURE LAND USE

The potential for land use change in the Uptown–Douglas (UD) area is huge, with policy directions signaling a much different condition than exists today. Changes in land uses, built form, transportation, facilities, parks and overall perception are just a

few of the shifts that are expected to occur, some sooner than others. The goal of this chapter is to ensure that changes in UD over the coming 20 to 30 years meet the community's Vision.

POLICIES

- 5.1.1 Evaluate applications for new development that seek changes to use, height and density in the UD area based on the land use and height designations identified on Map 5.1 and in Figure 5.1, as well as the policies contained in all sections of this Plan.
- 5.1.2 Support site specific changes to land use and/or height designations as per Map 5.1 and Figure 5.1 where developments advance overall plan objectives and include significant community contributions (see Sections 5.6 and 10.2).
- 5.1.3 Support developments that exceed the Base Building Height identified on Map 5.1 (but are within Maximum Building Height limit) provided that they:
 - Demonstrate advancement of objectives in all sections of the Plan, as appropriate to the site;
 - Provide community contributions as per rates identified in community contribution policy or agreed upon through a site-specific negotiation (see Policy 5.6.5); and
 - Align with the Plan's urban design framework (Section 9)
- 5.1.4 In addition to Policies 5.1.2 and 5.1.3, consider applications within the Core land use designation seeking to construct buildings taller than 18 storeys, provided proposed developments:
 - are a maximum of 24 storeys in height;
 - are located on or directly adjacent to the multi-modal Transit Hub site, on the half of the Uptown Shopping Centre site closest to Ravine Way / Carey Road, or on the half of Saanich Plaza site closest to Ravine Way. Other Core sites may be considered in exceptional circumstances;
 - demonstrate how increased building height can contribute to on-site open space and public realm improvements;
 - demonstrate application of good urban design specific to taller buildings, including massing, skyline character and shadowing impacts; and
 - prepare a land lift analysis specific to additional height above 18 storeys to inform negotiated community contributions (see Policy 5.6.6).
- 5.1.5 Apply building height guidelines identified in Figure 5.1 through:
 - Permitting a storey height based on current development standards, typically:
 - o For residential: 2.4 – 3.6 metres above finished floor;
 - o For ground floor commercial / industrial: 4.5 – 6.1 metres above finished floor; and
 - o For commercial / industrial storeys above the ground floor: 4.3 metres above finished floor.
 - Considering lofts/mezzanines as a separate storey; and
 - Considering the building height in storeys at all elevations in assessing consistency with guidelines.
 - Generally, permit increased building heights for new industrial units at grade, between 5.4m -7.3m, above finished floor.
- 5.1.6 As part of development proposals, achieve enhancements to the mobility network, including new connections identified in Section 6 and road dedications and streetscape improvements consistent with policies and cross-sections identified in Section 7.
- 5.1.7 Assess potential impacts of new development, including through requiring studies to analyze impacts on active transportation networks, parking, traffic flows, infrastructure capacity, ecological assets, land use and building transitions, views, shadowing and shading, and geotechnical conditions.
- 5.1.8 Encourage land assembly that avoids isolating individual parcels that may be subject to restricted development potential, including parcels with frontages of less than 30 metres in the Core and Urban Mixed-Use land use designations.
- 5.1.9 Generally do not support rezoning or variance applications for projects that have building heights that are significantly lower than the Base building height designations identified in this Plan.

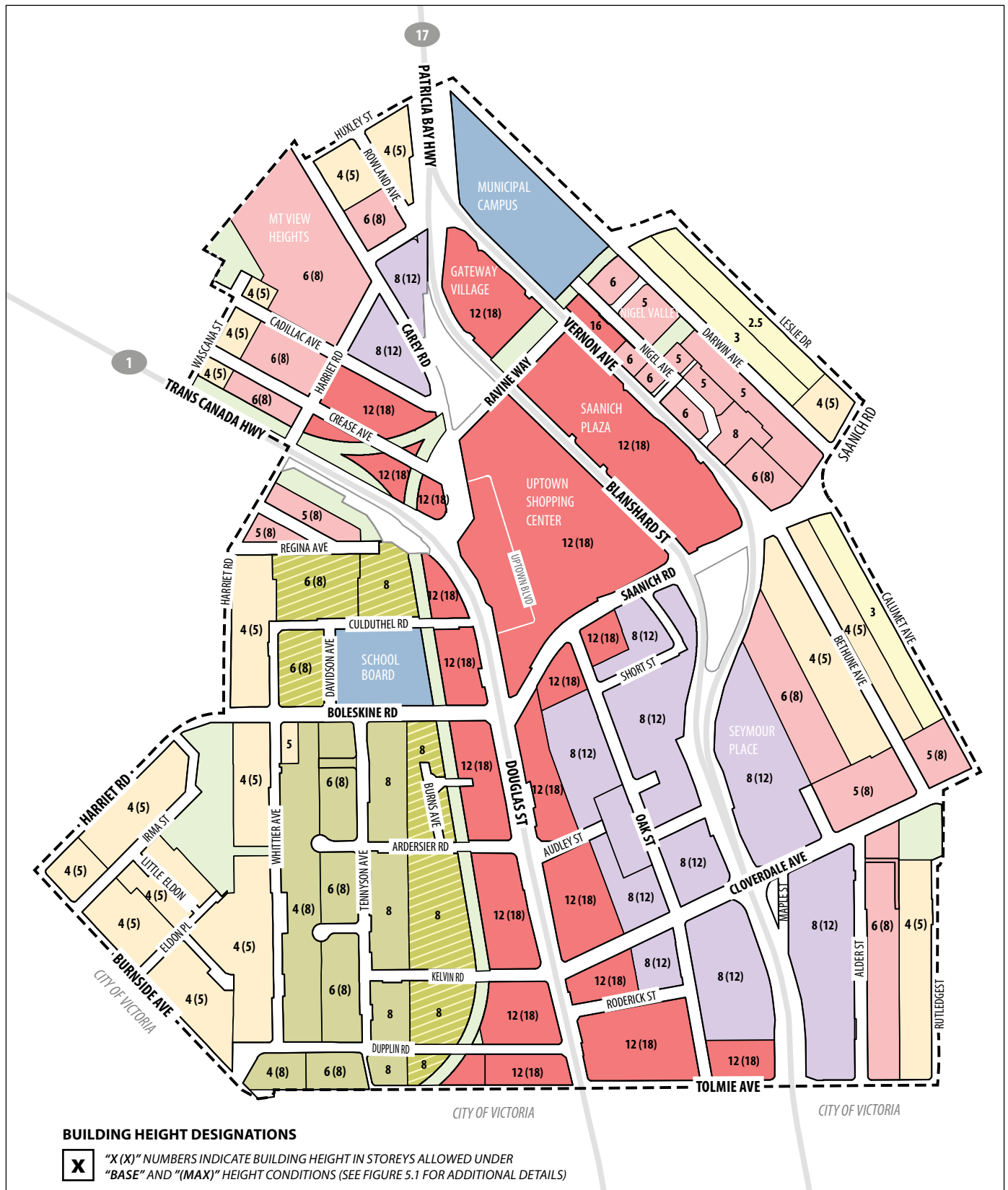


Figure 5.1 Uptown-Douglas Land Use and Building Height Designations

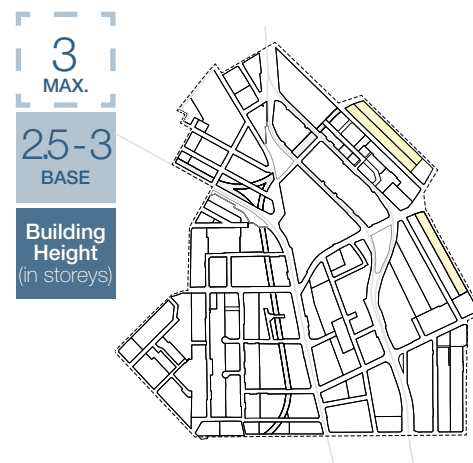
Neighbourhood Residential

Building Type and Use:

- Ground-oriented multi-unit residential buildings including houseplex, rowhouse and townhouse.
- Developments along Leslie Drive are restricted to 2.5 storeys.

Street Interface Guidelines:

- 4-6 metre front yard setback (see guideline 9.2.2 vi).
- Residential units must have direct access, frontage and views to the street.



Neighbourhood Apartment Residential

Building Type and Use:

- Multi-unit residential buildings including stacked townhomes, courtyard housing, and apartments.
- Limited neighbourhood commercial at grade.

Street Interface Guidelines:

- 4 metre front yard setback from property line to building face for residential.
- Provide a street wall height of 2-3 storeys.
- Buildings must stepback a minimum of 3 metres at street wall height.
- Residential typologies with a mix of direct street facing accesses, including entrances to individual units and notable shared entrances to multi-unit residences on upper storeys.
- Ground level units must have direct access, frontage and views to the street.

★ Not for profit affordable housing projects up to 6 storeys may be permitted on all properties designated Neighbourhood Apartment Residential.

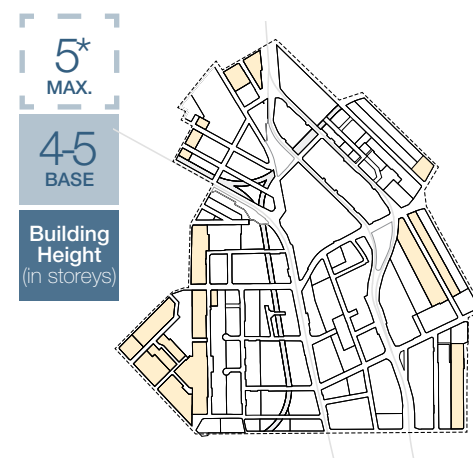


Figure 5.1 Uptown-Douglas Land Use and Building Height Designations

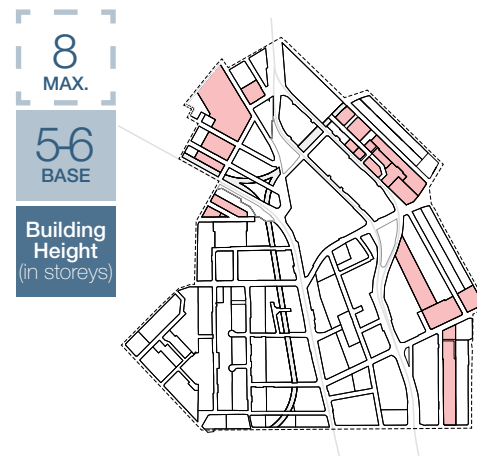
Mid-Rise Residential

Building Type and Use:

- Mid-rise apartment buildings.
- Commercial units at grade encouraged along Blanshard Street, Vernon Avenue, Cloverdale Avenue and Carey Road.

Street Interface Guidelines:

- 4 metre front yard setback from property line to building face for residential.
- Provide a street wall of 2-3 storeys (see Map 9.2).
- Buildings must stepback a minimum of 3 metres at street wall height.



Urban Mixed-Use Residential

Building Type and Use:

- Mid to high-rise mixed use and residential buildings.
- Active commercial at grade required along Oak Street, Audley Crossing and Cloverdale Avenue.
- Commercial uses will be considered for the first 4 storeys, with additional storeys along Blanshard Street.
- Live-Work units will be considered.

Street Interface Guidelines:

- 7-9 metre building setback from the outer edge of the curb to building face at grade (see guideline 9.2.2 vi).
- Provide a street wall of 2-4 storeys (see Map 9.2).
- 2 metre maximum setback from property line to building face for developments fronting Audley Crossing (see guideline 9.2.2 vi).
- Provide a street wall of 2 storeys along Audley Crossing (see Map 9.2).
- Buildings must step back a minimum of 3 metres at street wall height.
- Residential should include ground-oriented units with direct access, frontage and views to the street.
- Commercial units at grade should provide a range of small street frontages that animate the street.

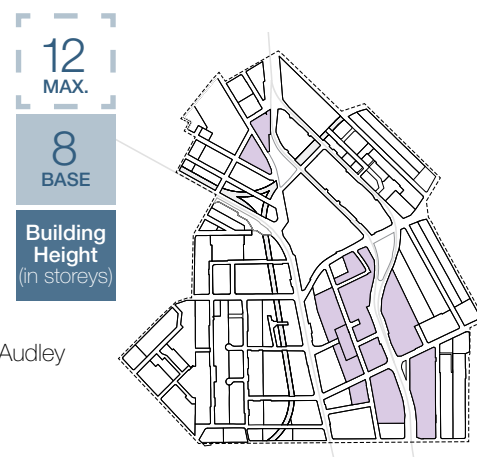


Figure 5.1 Uptown-Douglas Land Use and Building Height Designations

Core

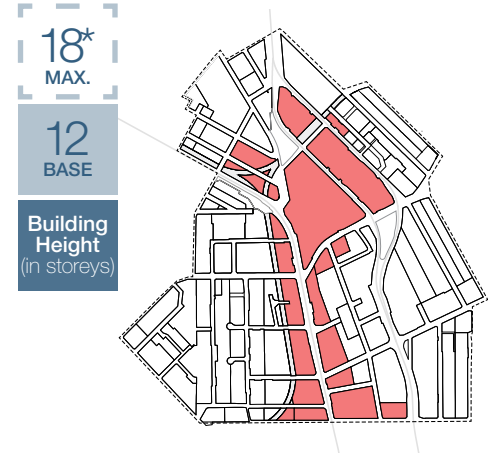
Building Type and Use:

- High-rise mixed-use or commercial buildings.
- Commercial at grade required.
- Residential and commercial uses permitted on the upper floors.

Street Interface Guidelines:

- Provide a street wall of 4-6 storeys. Lower street walls may be required on narrower streets (see Map 9.2).
- Buildings must step back a minimum of 3 metres at streetwall height.
- 5 metre setback from property line to building face along the Galloping Goose Regional Trail (see guideline 9.2.2 vi).
- Developments fronting along the Galloping Goose Regional trail must stepback at 2 storeys (see Map 9.2)
- Commercial at grade should provide a range of small street frontages and enhance the street.

★ In limited circumstances, building beyond 18 storeys may be considered. See policy 5.1.4.



Mixed Employment

Building Type and Use:

- Mixed-use buildings with light industrial, commercial and residential.
- Minimum 50% of first two storeys are required to be light industrial.
- Employment uses required at grade.
- Limited residential at grade.
- Live-Work units considered.
- Residential permitted in upper storeys, rental tenure strongly preferred.

Street Interface Guidelines:

- 5 metre setback from property line to building face along the Galloping Goose Regional Trail (see guideline 9.2.2 vi).
- Developments fronting along the Galloping Goose Regional trail must stepback at 2 storeys (see Map 9.2)
- Ground level uses must have direct access, frontage and views to the street.

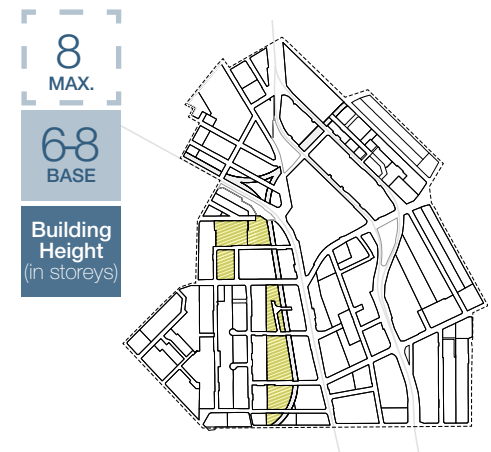


Figure 5.1 Uptown-Douglas Land Use and Building Height Designations

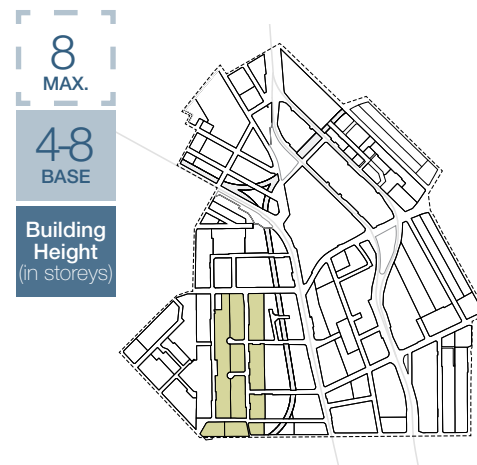
Tennyson Industrial

Building Type and Use:

- Light industrial buildings and structures.
- Commercial at grade will be considered for developments with frontages along Boleskine Road, Tennyson Avenue and Dupplin Road.

Street Interface Guidelines:

- Buildings with commercial uses must provide outdoor areas with landscaping, seating and weather protection.
- Ground level uses must have direct access, frontage and views to the street.



Mixed Institutional

Building Type and Use:

- Typically municipal, provincial or federal buildings including schools, community facilities, commercial mixed-use and recreation.

Street Interface Guidelines:

- Developments must engage the street, provide prominent public open spaces, and be fronted by active uses.

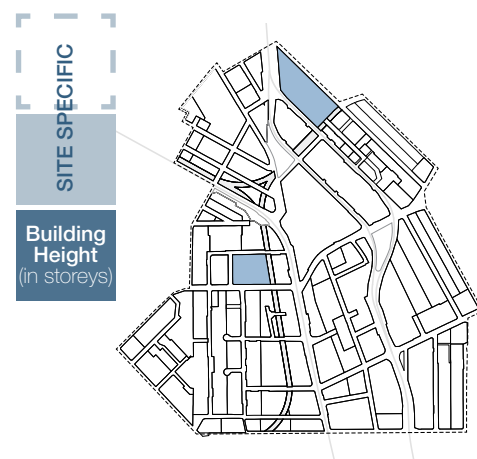


Figure 5.1 Uptown-Douglas Land Use and Building Height Designations

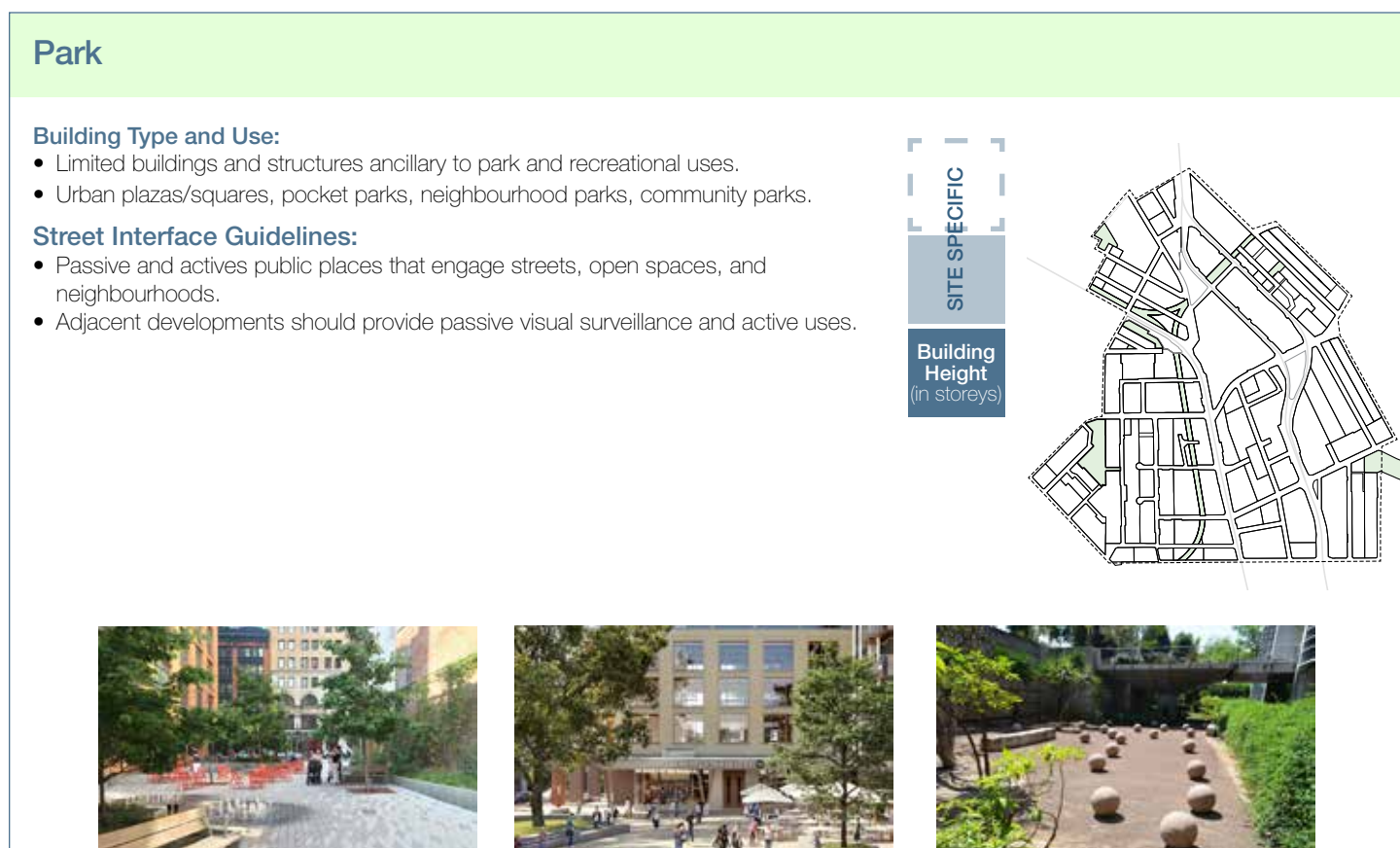


Photo Sources:

Neighbourhood Residential:

- Hollingdale Mainwaring Architecture: *Vida North Lake*
- Mosaic: *Fremont Blue*
- B Squared Architecture: *Passive House Duplex*

Neighbourhood Apartment Residential:

- Abstract: *4 storey Bowker*
- PNG Architects, *Hill East*
- AA Studio: *King + Sullivan Townhomes*

Mid-Rise Residential:

- Abstract: *Black and White*
- Aragon: *Platform*
- Quadrangle Architects: *Duke Condos*

Urban Mixed-Use Residential:

- Curated Properties & Windmill: *The Plant*
- BTL Architects: *East Market*
- Gensler & HWKN: *25 Kent Ave.*

Core:

- Sweeny Sterlin Finlayson and Co Architects: *Liberty*
- HKS: *VESPR*
- Chris Dikeakos Architects & Millennium Development Group, City of North Vancouver

Mixed Employment:

- Westbank & Henriquez Partners Architects, Vancouver
- GBL Architects: *The Workspaces @ Strathcona*
- Opus: *Titan*

Tennyson Industrial:

- Wesgroup: *22FOUR*
- CHP Architects: *Lustre Development*
- Conwest Group: *Ironworks*

Mixed Institutional:

- Detroit: *Campus Martius Park*
- Northern Sky Architecture: *St. Vital*
- Moriama & Teshima Architect & Acton Ostry Architects: *The Arbour*

Park:

- NYC: *Beekman Plaza*
- Aragon: *Esquimalt Town Square*
- Jardines Biblioteca Vasconcelos, Mexico City

Note: The images selected are for visual reference to building type, use and design but are not necessarily in alignment with the policies and guidelines contained in this Plan.

5.2 HOUSING

The provision of housing types that can accommodate people of different ages, incomes, family structures, and physical and social needs is one of the fundamental elements of creating a healthy, inclusive and sustainable community. The Rudd, Rutledge, Mt. View, and Nigel Valley-Municipal Campus sub-area neighbourhoods are ideal locations for new low and medium-rise housing forms that are family-appropriate (e.g. more bedrooms, larger private outdoor space). Recent analysis completed as part of the Regional Housing Affordability Strategy (2018) shows that there is a shortfall of approximately 20,000 rental units in the region for households with “very low” to “low to moderate” incomes. This need is particularly acute for renters, where 45% of renters pay more than 30% of their income on housing (compared with 19% of owners).

Saanich's Official Community Plan and Housing Strategy encourage a range of housing types to address community housing needs and to help build a complete community. Lack of housing supply and diversity are evident in the Uptown-Douglas area, including rental units, affordable units, family-oriented housing, smaller units, and ground-oriented attached housing units. Section 10.2 addresses affordable and supportive housing.

A capacity analysis prepared for the Uptown-Douglas (UD) Plan area estimates that the number of new units developed to be 7-10 times that of today. The projected growth of the UD area combined with its proximity to a range of transportation options, shops and services make it an ideal location to accommodate new housing stock. The region's high housing costs, low vacancy rates, and modest variety of supply all pose challenges to adequately house people. Denser sub-areas closer to Douglas Street will provide housing suitable for a range of households in a location where car ownership is not essential and at price points that potentially are lower than many other areas of Saanich and the region. A critical measure of success for this Plan will be the ability to accommodate a diversity of housing, including rental and non-market housing forms.

This section provides guidance for the development of new housing and is intended to work in tandem with Section 10.2, which focuses on supportive and affordable housing.



The 881, Short Street



Private outdoor space, The 881, Short Street

POLICIES

- 5.2.1 Expand housing choices in the UD area through supporting a diversity of residential densities and development forms (see Map 5.1 and Figure 5.1).
- 5.2.2 Target a minimum of 30% of new residential uses to include two and three-bedrooms units as a means of providing family appropriate housing (see Policy 10.2.8).
- 5.2.3 Support developments that are non-market or include an affordable housing component (see Policy 10.2.4) through considering:
- Additional density and building heights;
 - Parking variances;
 - Financial support through the Saanich Affordable Housing Fund;
 - Partial waivers (up to 50%) to Development Cost Charges;
 - Property tax exemptions; and
 - Funding through grant programs.
- 5.2.4 Encourage non-market housing as a component of new residential developments.
- 5.2.5 Work in partnership with the Capital Regional District, BC Housing and other agencies to support affordable housing developments, including identifying potential affordable housing sites.
- 5.2.6 Support live/work residential units at grade to expand employment opportunities and local ownership and animate higher density residential areas.
- 5.2.7 Require new residential developments to provide useable private outdoor space, not less than 6m² per unit for apartments or 9m² for townhouse and rowhouse units, or common usable outdoor space equivalent to the aggregate individual unit requirement. Common space should include quality play areas, gardens and social areas with seating and weather protection.
- 5.2.8 Support innovative infill housing solutions, with preference given to affordable housing developments, which offer a range of unit size and type (e.g. micro and accessible).
- 5.2.9 Promote child-friendly communities through encouraging larger unit sizes, building/development amenities, and neighbourhood programming that focuses on the needs of children and youth;
- 5.2.10 Encourage new residential developments to include engaging child-friendly design elements within common spaces.
- 5.2.11 Support the retention of existing purpose built rental apartments and tenants, including through:
- Providing incentives for renovations and energy upgrades;
 - Consideration of the replacement of rental units at similar unit types (# of bedrooms) and price points in the new development; and
 - Developing a tenant assistance policy to support displaced tenants.
- 5.2.12 Support implementation of Housing Strategy “missing middle” housing initiatives through implementation of the land use designation and buildings heights in this Plan.

5.3 COMMERCIAL AND COMMERCIAL MIXED-USE

A key objective for the Uptown–Douglas (UD) area is to retain and increase employment within the area.

A significant supply of commercial already exists, including Uptown Shopping Centre, Gateway Village, Island Home Centre and Saanich Plaza. To strengthen existing commercial areas, this Plan also encourages increased densities. With higher densities, existing expansive commercial areas will transition to a built form that is more consistent with dense urban areas, including underground parking, integration of residential uses and street-oriented design.

In addition, there is a significant amount of auto retail, an industry that is undergoing a shift in how products are sold

and distributed. Auto dealers are beginning to experiment with alternatives to traditional sales, including near online-only and direct sales. This shift aligns with the dramatically altered land use plan envisioned within the Douglas-Oak Hub. While these industries will continue to exist in the near term, the longer term vision for the area does not include space intensive, vehicle-oriented uses.

The existing commercial focus of the area will be enhanced, further strengthening the supply and quality of retail and office offerings. The substantial increase in residential will expand the customer and employee base within walking distance, further enhancing the resiliency of area businesses.



Bin 4

POLICIES

- 5.3.1 Encourage commercial and mixed-use buildings to include small-scale commercial/retail uses at street level, with larger retail uses either located above or behind at fronting uses (see also Guideline 9.2.8 xi. and Figure 9.14).
- 5.3.2 Support mixed-use developments on commercial streets and in high pedestrian traffic areas to promote an environment that is hospitable and encourages social interactions.
- 5.3.3 Support live/work units and co-work/worker-share spaces as part of mixed-use developments within the Tennyson Industrial Quarter, Tolmie Quarter and the Douglas-Oak Hub neighbourhood sub-areas.
- 5.3.4 Expect developments to create small, attractive storefronts that define the building and add variation to its facade that cue visual interest at grade.
- 5.3.5 Promote a range of commercial uses that offer a variety of services for all ages and stages of life, including child-care, medical services and professional offices.
- 5.3.6 Do not support the expansion of traditional auto retail models of dealerships (with on-site vehicle storage beyond a showroom), including the expansion of established businesses.

5.4 INDUSTRIAL LANDS

The Uptown-Douglas (UD) area contains a significant portion of Saanich's industrial lands and represents a strategic location for many businesses due to convenient access to regional destinations and travel routes. While the area does not have an abundance of large sites, it has a diverse range of business types.

This Plan emphasizes the importance of retaining industrial lands while recognizing movement away from traditional industrial forms towards lighter, multi-purpose forms, such as the technology sector. The retention of industrial uses is emphasized in the Tennyson Industrial Quarter. This, along with added industrial opportunity in the Tolmie Quarter neighbourhood sub-area will ensure the existing integrity of the land base is maintained. Broader uses above industrial are permitted in these areas which will enable continued investment and enhance the overall vibrancy of this unique and valued area.



Galloping Goose Regional Trail

POLICIES

- 5.4.1 Maintain industrial as the primary use for all developments in lands designated as Tennyson Industrial.
- 5.4.2 Require light industrial to be a component of all developments in areas designated as Mixed Employment (See Figure 5.1 - Mixed Employment).
- 5.4.3 Support live/work units and co-work/worker-share spaces as part of mixed-use developments within the Tennyson Industrial Quarter and Tolmie Quarter sub-areas.
- 5.4.4 Expect frontage improvements as part of industrial projects to ensure the development of a streetscape consistent with an urban area, including the introduction of separated sidewalks and street trees.
- 5.4.5 Support opportunities to introduce active uses and public realm improvements that animate the Galloping Goose Regional Trail and enhance connections to the industrial area.
- 5.4.6 Consider opportunities for commercial at grade for developments with frontages along Boleskine Road, Tennyson Avenue, and Dupplin Road.
- 5.4.7 Encourage industrial buildings to be designed and constructed to allow for future flexibility of the space.

5.5 INFRASTRUCTURE

Underground infrastructure is a critical component of a complete well-functioning community. Saanich's underground infrastructure includes sanitary sewers, storm drains, and water servicing. Saanich evaluates the capacity of its underground services as a part of the redevelopment process.

In partnership with the Capital Regional District's core area waste water treatment plant, Saanich has been allocated a quota for use of the sewage system. Currently, there is capacity to add new development within that quota. Eventually, Saanich will reach its limit, and new development will need to attenuate (hold back) sewage during peak times, and pump when there is available capacity.



Skyline looking north-west, Douglas-Oak Hub

POLICIES

- 5.5.1 Continue to review and update information on infrastructure and ensure adequate water, sewer and drainage capacity is available to support new development.
- 5.5.2 Consider the impacts of new development proposals on the overall sewer system. Optimize the efficiency of the network, including through requiring the use of sewage attenuation tanks where needed.
- 5.5.3 Work towards correcting non-conforming utility corridors where they exist.
- 5.5.4 Permit statutory rights of way for watermain looping or other infrastructure purposes.
- 5.5.5 Generally require all new developments to underground utility lines and pole-top equipment, as per BC Hydro's guidelines.
- 5.5.6 Further to Policy 5.5.5, should undergrounding of utilities not be achievable in the near term due to parcel characteristics, require installation of pre-ducting on the property and the provision of bonding to enable future undergrounding of utilities when adjacent properties redevelop.
- 5.5.7 Explore the use of latecomer agreements that would enable utilities to be undergrounded at the block level prior to redevelopment of all sites on that block.
- 5.5.8 Continue to work with BC Hydro to develop a supported list of standard planting specifications and vegetation maintenance for overhead and underground utilities.

5.6 COMMUNITY CONTRIBUTIONS

To help deliver new community facilities and infrastructure vital to the well-being of residents, new developments are encouraged to contribute to neighbourhoods where change is occurring. These community benefits are sometimes built within a new project – such as a childcare facility or cultural space, or in other cases a developer makes a payment in lieu so that funds from a number of projects can be pooled to fund improvements. Community contributions supplement Development Cost Charges (DCCs), which are fixed rate contributions that help finance growth.

Typically, rezoning proposals involve an individual negotiation with the applicant to determine appropriate community contributions. This section looks to provide clarity on principles, priority areas and expectations for community contributions in the Uptown–Douglas (UD) area. In negotiating community contributions, the intent is to deliver improvements, such as public plazas or pocket parks, that directly benefit the area, as well as improvements, such as affordable housing or building energy performance that address broader municipal goals, beyond development requirements/improvements identified in other bylaws.

POLICIES

- 5.6.1 Seek community contributions in the UD based on the following principles:
- Ensure new developments contribute facilities and infrastructure that will support the well-being of residents, employees and visitors both at the site level and in the broader area;
 - Consider the broader goals of climate action, housing affordability and biodiversity when assessing community contributions;
 - Ensure the extent of community contributions are commensurate with the scale of proposed development / additional density;
 - Where possible, address on-site assets and opportunities, such as the preservation of heritage buildings, enhancement of natural features or addition of pedestrian / cycling connections;
 - Endeavour to make the community contribution negotiation process consistent, transparent and clear; and
 - Consider affordable housing as a core component of a complete community.
- 5.6.2 Require community contribution statements for all rezoning applications.
- 5.6.3 Prioritize affordable housing, followed by new parks/open space (non-DCC), and public realm enhancements, when negotiating community contributions.
- 5.6.4 Seek community contributions as part of the redevelopment process, with a consideration for the following items:
- Affordable housing (See Policy 5.6.3 and Section 10.2);
 - Parks or plazas (See Policies 5.6.3 and 8.1.5);
 - Public realm improvements (See Policy 5.6.3);
 - Dedications or easements that create new connections for pedestrians and cyclists;
 - Daycare facility, with an affordability component;
 - Significant sustainable building features;
 - Protection or enhancement of natural areas, including Cecelia Creek;
 - Publicly accessible green roofs with amenity value;
 - Public Art;
 - Community Facilities;
 - Park improvements; and
 - Cash contributions to local improvements.
- 5.6.5 Undertake an analysis to enable the development of a structured amenity policy and contribution rate for rezoning applications that would establish a transparent and predictable system for obtaining community amenity contributions in the UD Plan area.

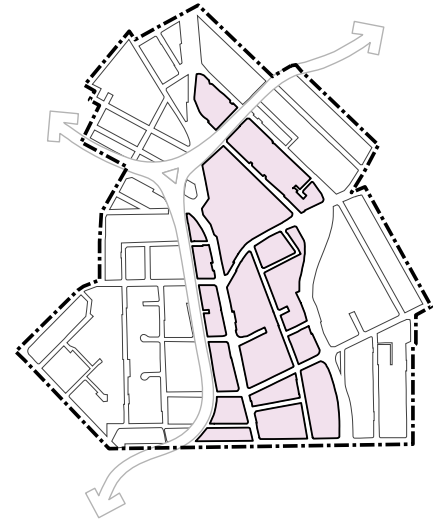


Rutledge Park

- 5.6.6 For developments that exceed the maximum height in the Core designation (see policy 5.1.4), as identified on Map 5.1, require the provision of a pro forma analysis to determine the value of property land lift, and seek to acquire 75% of the land lift for community contributions.
- 5.6.7 Explore community contributions for rental housing as part of the analysis for establishing a fixed rate amenity cost.

5.7 DOUGLAS - OAK HUB

The heart of activity in the Uptown-Douglas area is in this sub-area, as it contains the majority of commercial uses and major transportation corridors. In future, this area will see significant intensification, including more housing, which will balance out the high concentration of commercial uses and support the long term introduction of rapid transit.

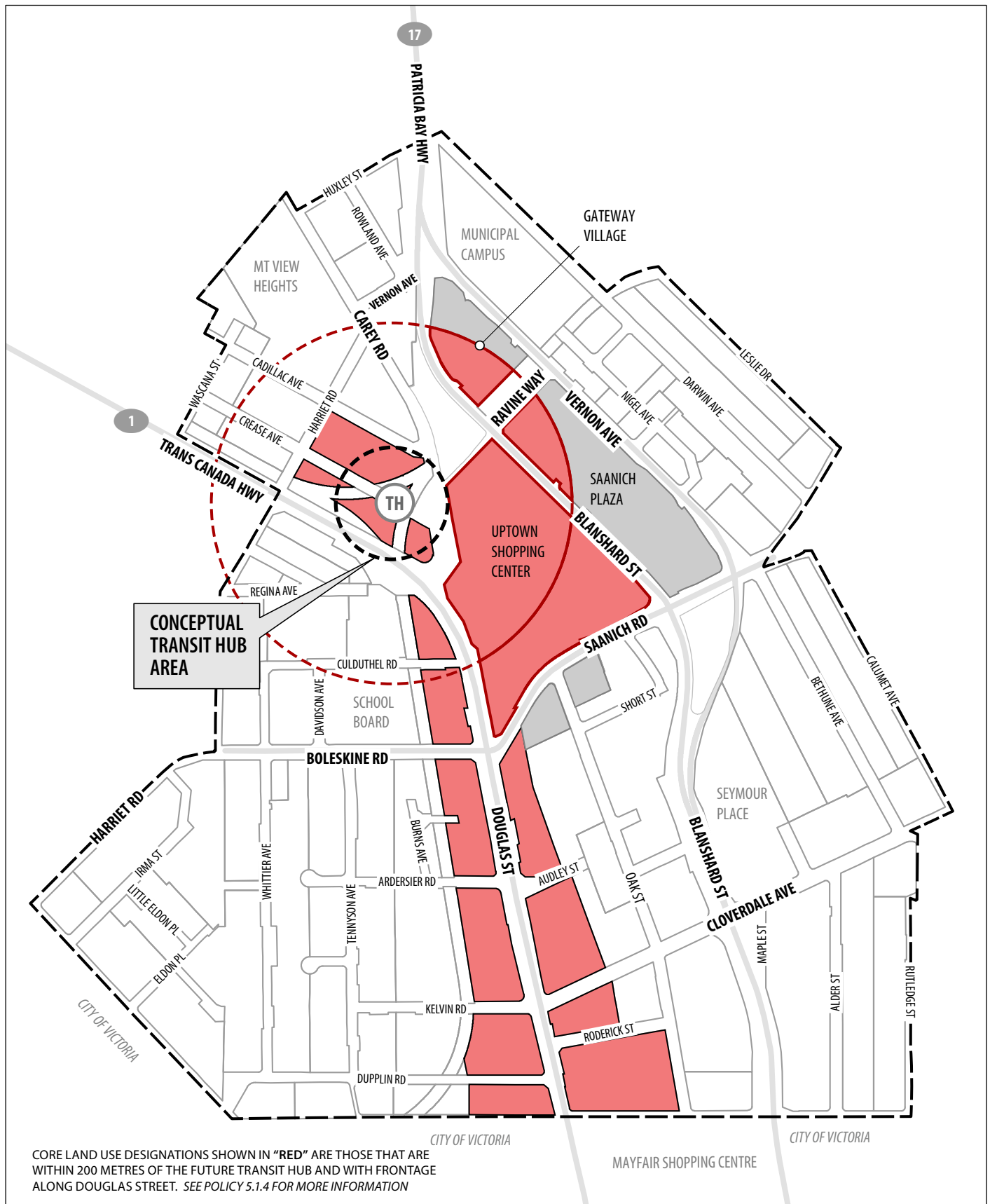


SUB-AREA OBJECTIVES

- A. Emphasize Douglas Street as the primary commercial and transit corridor in the region.
- B. Support the introduction of uses that reinforce the area as the heart of Saanich and enhance the range of destinations and cultural experiences.
- C. Design Oak Street as a special street with a blend of commercial activity and medium to high density residential.
- D. Integrate major parks and a variety of public open spaces and green spaces as a key component of infrastructure needed to support higher density development.
- E. Support opportunities to locate active commercial uses fronting the Galloping Goose Regional Trail.
- F. Enhance walkability and the pedestrian experience by reducing the impact of major roads and adding connections to the street grid.



Looking South from Uptown Shopping Centre



CONCEPCTUAL
TRANSIT HUB AREA



CORE DESIGNATED AREA
(WITHIN POLICY 5.1.4)



CORE DESIGNATED AREA
(OUTSIDE POLICY 5.1.4)

POLICIES

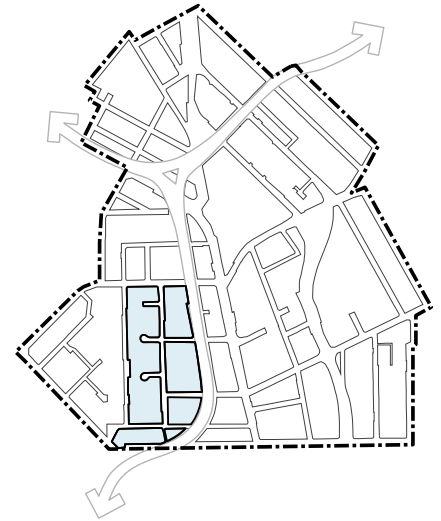
- 5.7.1 Support new development that enhances the position of the area as a primary employment, commercial and community hub.
- 5.7.2 Support rezoning applications along Douglas Street that accommodate a strong concentration of employment and commercial uses along with complementary uses such as multi-unit developments, hotels, restaurants, public institutions, personal service businesses and retail stores.
- 5.7.3 Support high density commercial buildings along Douglas Street to make efficient use of infrastructure and to maintain compact building footprints.
- 5.7.4 Concentrate the tallest buildings along the Douglas-Oak Hub and transition downwards to surrounding areas to the east and west.
- 5.7.5 Properties on the west side of Douglas Street that are adjacent to the Galloping Goose Regional Trail are expected to provide frontages that enhance the trail through design features (e.g. building design, site landscaping, pocket parks) and building massing that steps down to the trail (see also Figure 5.1, Policy 8.3.9, Guidelines 9.2.2 vi, and Map 9.2).
- 5.7.6 Strongly discourage large blank walls, chain-link fences, and surface parking for properties fronting along the Galloping Goose Regional Trail.
- 5.7.7 Support medium to high-density mixed-use developments along Oak Street that enhance neighbourhood livability and expand the range of housing options.
- 5.7.8 Ensure development on Oak Street maintains a human scale including through setting back upper storeys, restricting surface parking and discouraging blank walls and larger building faces fronting the street.
- 5.7.9 Encourage redevelopment of the old Mayfair Lanes site (760 Tolmie Avenue) with a mix of uses that incorporates public open space, active uses and a significant residential component.
- 5.7.10 Support the creation of a Master Plan for the Gateway Village and Saanich Plaza sites that includes higher density redevelopment as well as the integration of pathways and roads to create better connectivity and pedestrian-oriented streetscapes and the integration of a prominent public open space.
- 5.7.11 Support changes that better integrate the Uptown Shopping Centre into the fabric of the neighbourhood through public realm improvements, wayfinding and improved connections for all modes.
- 5.7.12 Expect development proposals to identify how they are addressing green space and the urban forest on their sites, including through the provision of parks and open space, green roofs and streetscape improvements (See also Section 8).
- 5.7.13 Add a new park space along Oak Street and for Audley Crossing to serve future residents and employees, orient future development onto the new park space and ensure it is well connected and easily accessible to the surrounding neighbourhood (See also Policy 8.1.3).



Looking South towards Oak Street and Blanshard Street

5.8 TENNYSON INDUSTRIAL QUARTER

The Tennyson Industrial Quarter is a valuable employment area in Saanich and houses an eclectic mix of businesses. Commercial uses, secondary to industrial, are important to economic vibrancy of the Quarter. The intent of this sub-area is to provide a broad range of opportunity, generally with light industrial at grade and ancillary commercial uses above.



SUB-AREA OBJECTIVES

- A. Retain industrial uses and support opportunities that increase density (floor space) and the number of jobs.
- B. Expand opportunities animate the Galloping Goose Regional Trail.
- C. Support mixed-use developments adjacent to the Galloping Goose Regional Trail, combining industrial, commercial and residential uses. Enhance goods movement and reduce conflicts between modes.
- D. Enhance streetscape conditions to improve the quality of the area and broaden its attractiveness to a range of employment uses.
- E. Enhance goods movement and reduce conflict between all modes of transportation.



Home Lumber Office - 470 Ardersier Road



Map 5.3 Tennyson Industrial Quarter

POLICIES

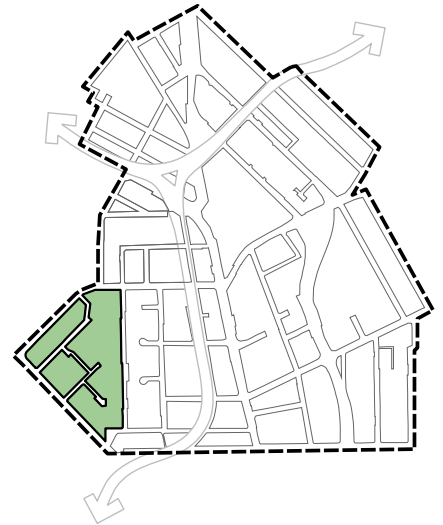
- 5.8.1 Support density increases that provide additional employment opportunities and protect and enhance the industrial character of the area (See also Policy 11.1.8).
- 5.8.2 Require 50% of the first two storeys in new development to be industrial and/or light industrial in nature.
- 5.8.3 Permit live/work units along Boleskine Road and Tolmie Lane.
- 5.8.4 Support stacked strata and mixed-use developments, including industrial, commercial and residential uses, in the Mixed Employment designation.
- 5.8.5 Work with the Capital Regional District to expand opportunities for animated uses along the Galloping Goose Regional Trail, such as the development of an Artisan Alley that includes live/work studios, light industrial uses and food establishments.
- 5.8.6 Pursue the development of pocket parks or plazas along the Galloping Goose Regional Trail, including near the intersection of Dupplin Road or Tolmie Avenue (See also Policy 8.3.3).
- 5.8.7 Through redevelopment, encourage Heritage Designation of the property located at 470 Ardesier Road (currently listed on the Heritage Register).
- 5.8.8 Improve the streets by adding sidewalks and street trees as part of local improvements and development changes to increase its attractiveness to existing and future employers.
- 5.8.9 Implement frontage improvements to Tolmie Lane as per the cross-section for Audley Crossing (see Section 7.9 and Figure 7.10), including through the redevelopment process.
- 5.8.10 Properties that are adjacent to the Galloping Goose Regional Trail are expected to provide frontages that enhance the trail through design features (e.g. building design, site landscaping, pocket parks) and building massing that steps down to the trail (see also Figure 5.1, Policy 8.3.9, Guidelines 9.2.2 vi, and Map 9.2).



Tennyson Avenue

5.9 RUDD NEIGHBOURHOOD

The Rudd Neighbourhood contains a mix of single detached dwellings and multi-unit housing, with Burnside Road at the west forming a border with the City of Victoria. Over time, the area is envisioned to provide additional options for housing through low-rise apartments near Rudd Park.

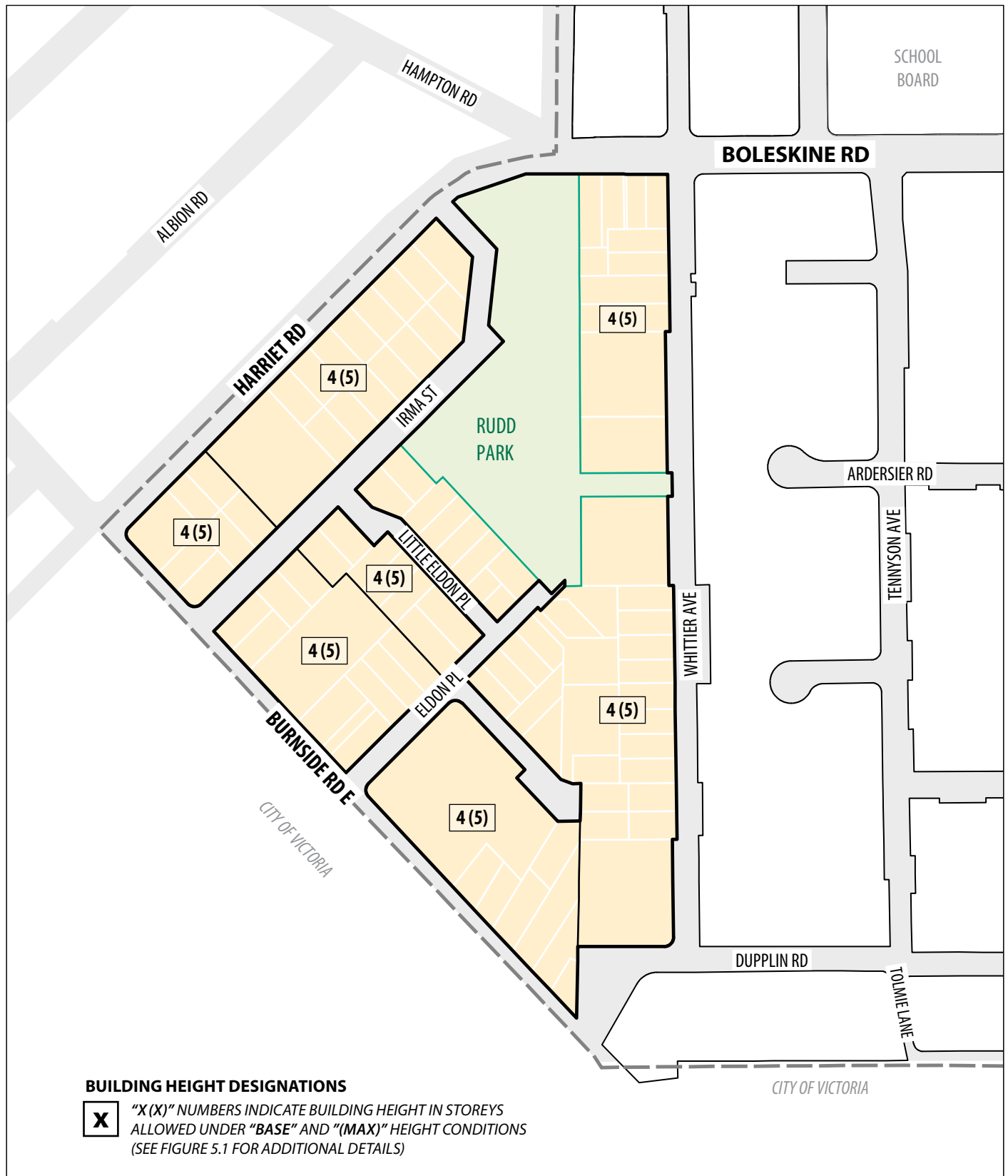


SUB-AREA OBJECTIVES

- A. Support family-appropriate housing choices in all new residential development.
- B. Explore opportunities for medium density residential on Burnside Road to complement City of Victoria land use designations.
- C. Continue to use Rudd Park as an amenity to anchor multi-unit residential housing developments.
- D. Explore opportunities to enhance/expand Rudd Park.
- E. Protect, rehabilitate, and restore heritage assets.



Rudd Park



POLICIES

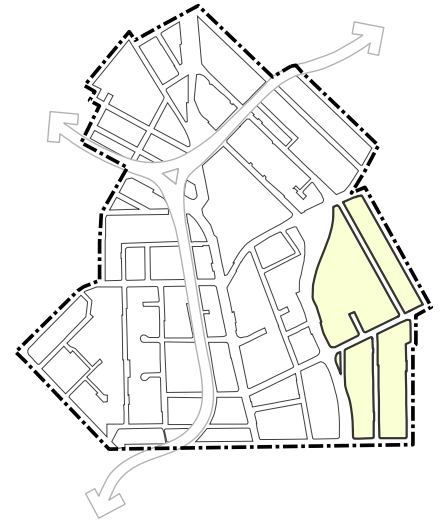
- 5.9.1 Permit apartment residential development on Burnside Road to complement City of Victoria land uses on the other side of the road.
- 5.9.2 Enhance Whittier Avenue as a neighbourhood greenway through introducing separated sidewalks at the time of redevelopment, further enhancing the green buffer on the east side of the street and minimizing impacts from vehicle travel.
- 5.9.3 Through redevelopment, work with the City of Victoria in determining the desired right-of-way for Burnside Road East from Harriet Road to Dupplin Road.



452 Boleskine Road

5.10 RUTLEDGE NEIGHBOURHOOD

The Rutledge Neighbourhood is centred on Rutledge Park and the recent developments that have sprung up around the Park. Over time, the redevelopment of the property at 4000 Seymour will provide significant opportunities to better connect to the broader Uptown–Douglas (UD) area and add amenities and housing to the area.

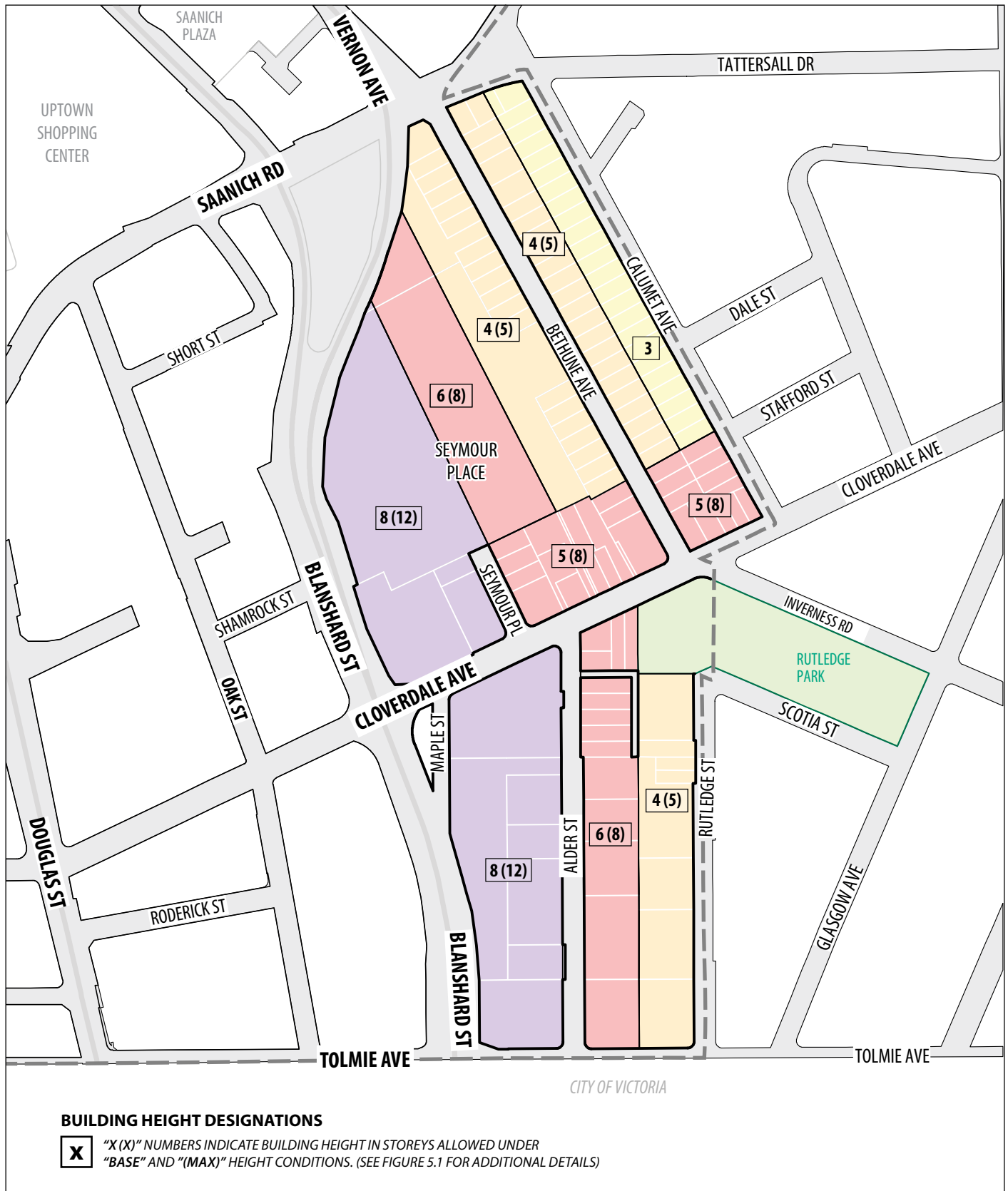


SUB-AREA OBJECTIVES

- A. Expand the range of housing options.
- B. Improve the connectivity of the sub-area for pedestrians and cyclists.
- C. Improve connections between the Galloping Goose Regional Trail and Rutledge Park along Cloverdale Avenue through public enhancements and street-oriented developments with active uses.
- D. Support higher density development on the 4000 Seymour site with integration of community uses and spaces and an improved mobility network.



Rutledge Park



POLICIES

- 5.10.1 Support the development of a master plan for the 4000 Seymour (4000 Seymour Place) site that incorporates a mix of residential and commercial uses, higher density buildings with smaller footprints, a fine-grained, walkable development pattern connected with the surrounding neighbourhood, limited surface parking, strong east-west connectivity from Blanshard to Bethune, community amenities and pedestrian-oriented green space.
- 5.10.2 As part of redevelopment projects, locate new building entrances on Blanshard Street and separate sidewalks with a treed boulevard to support the transition to a complete street.
- 5.10.3 Support ground floor retail or other active commercial uses on Cloverdale Avenue to animate the street and emphasize the connection to Rutledge Park.
- 5.10.4 Encourage small commercial frontages along Cloverdale Avenue as a means to support local commercial retail uses.
- 5.10.5 As part of redevelopment of 4000 Seymour Place, pursue the acquisition of a Neighbourhood Park that provides a recreational amenity to support new multi-unit development.
- 5.10.6 Work with the Ministry of Transportation and Infrastructure to explore potential options to utilize the highway island on Blanshard Street for public benefit.



Inverness Road

5.11 MT. VIEW NEIGHBOURHOOD

The Mt. View Neighbourhood is primarily residential, and includes Mt. View Park, the Campus of Care on Carey Road and some industrial land close to the Switchbridge, where the Galloping Goose and Lochside Regional Trails connect. The future introduction of the Transit Hub near the Switchbridge will add significant activity to the area and influence land use over the longer term. New housing opportunities in proximity to the hub will provide homes in a compact, complete community with abundant transit options.

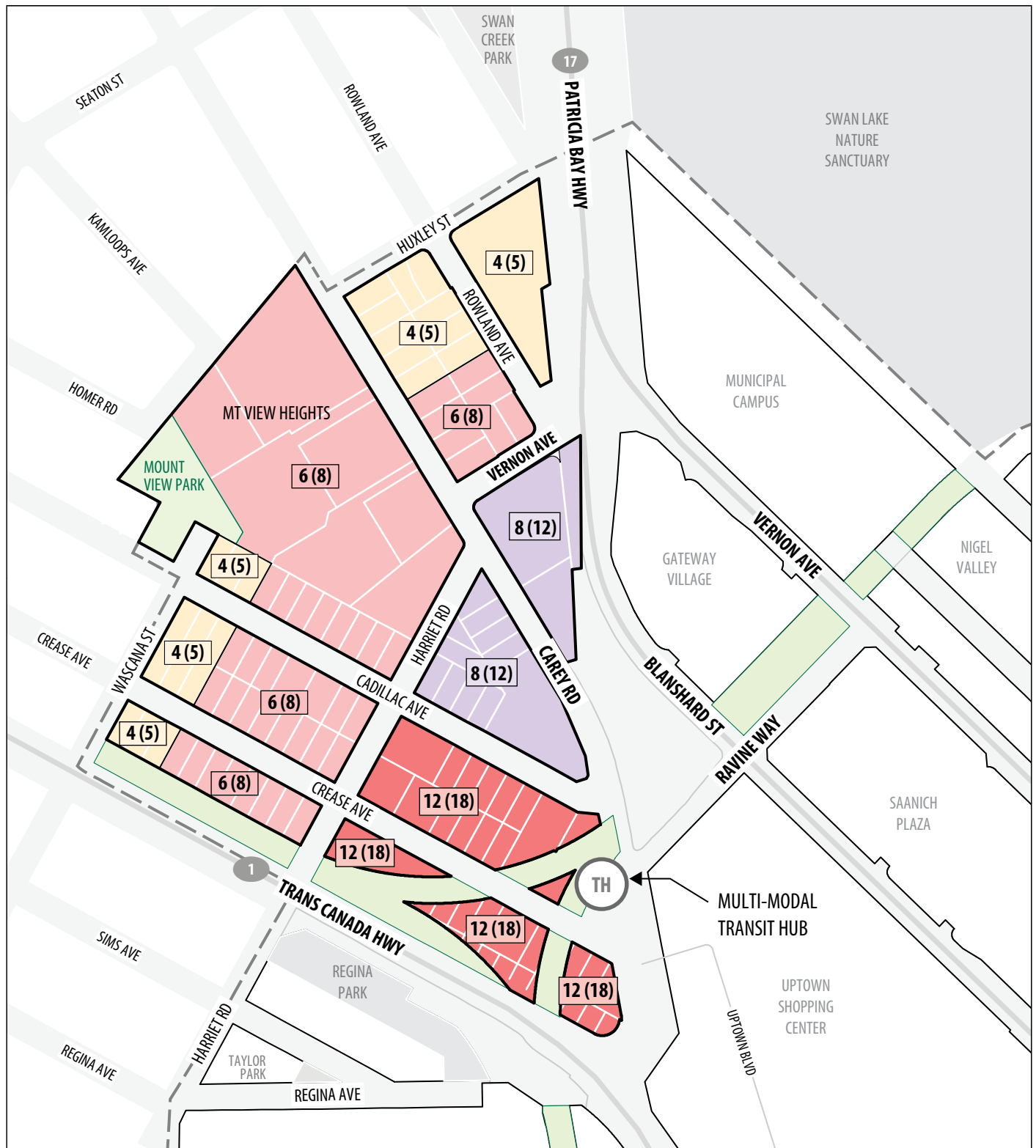


SUB-AREA OBJECTIVES

- A. Create a landmark multi-modal transit hub.
- B. Ensure the multi-modal transit hub integrates a mix of uses and promotes multi-modal connections and pedestrian activity.
- C. Support higher density uses and a diversity of housing within walking distance of the multi-modal transit hub while protecting viewscapes.
- D. Explore opportunities to create public space within the Ministry of Transportation and Infrastructure right-of-way green space.








Mount View Heights



BUILDING HEIGHT DESIGNATIONS

X "X(X)" NUMBERS INDICATE BUILDING HEIGHT IN STOREYS ALLOWED UNDER "BASE" AND "(MAX)" HEIGHT CONDITIONS (SEE FIGURE 5.1 FOR ADDITIONAL DETAILS)

LAND USE DESIGNATIONS

 NEIGHBOURHOOD APARTMENT	 URBAN MIXED-USE	 PARK / TRAIL
 MID-RISE RESIDENTIAL	 CORE	

POLICIES

- 5.11.1 Support BC Transit in the creation of a landmark multi-modal transit hub at the intersection of the Lochside Regional Trail and Galloping Goose Regional Trail that:
- Enhances the role and identity of the UD area as a regional hub and focal point in Saanich;
 - Includes attractive façades, public art, appropriate lighting and gathering spaces;
 - Creates a seamless and inviting experience for all modes, particularly pedestrians, cyclists and transit users;
 - Maintains the 'AAA' (all ages and abilities) quality of the Galloping Goose and Lochside Regional Trails;
 - Integrates high quality public spaces throughout the exchange;
 - Enables efficient transit access and egress along Douglas Street;
 - Accommodates the potential future conversion to light rail transit;
 - Incorporates active uses, housing and/or community facilities such as retail-commercial and a community centre, library or other similar services; and
 - Serves as a prominent visual landmark that highlights the area and serves as a point of orientation in the landscape.
- 5.11.2 Support the exploration of various land assembly configurations for the multi-modal transit hub to best achieve the items identified in Policy 5.11.1. Should land assemblies change the configuration of development parcels and transportation corridors, generally apply the Core land use designation to newly created development parcels in the immediate area.
- 5.11.3 Explore options to incorporate an underground transit hub system for the future BC Transit station, potentially connecting Vernon Avenue, Blanshard Street and Douglas Street below grade.
- 5.11.4 Support redevelopment that applies the principles of transit-oriented development, with provision for reduced parking requirements given the proximity to the future transit hub and active transportation facilities.
- 5.11.5 Explore enhancements along Wascana Avenue that provide a stronger connection between the Galloping Goose Regional Trail and Mt. View Park, including pedestrian and cycling improvements and the addition of landscaping and street trees.
- 5.11.6 Explore the addition of a pedestrian and cycling connection across the Pat Bay Highway to connect the Mt. View Neighbourhood sub-area to Swan Lake.



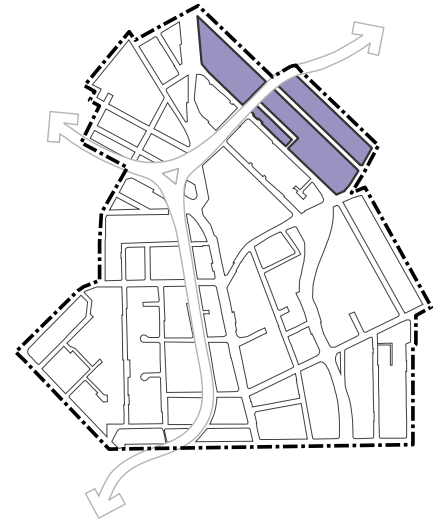
Uptown Place, Rowland Avenue



5.12 NIGEL VALLEY – MUNICIPAL CAMPUS

The Nigel Valley - Municipal Campus sub-area contains two key sites within the area, namely the Nigel Valley master-planned site and the Municipal Campus, including the Fire Hall, Police Department, and Municipal Hall.

The redevelopment of the Nigel Valley site will change the character of this sub-area and create high quality housing for vulnerable members of society. Longer term, the redevelopment of the Municipal Campus site will enable important public institutions to be better connected to the area.



SUB-AREA OBJECTIVES

- A. Highlight the Municipal Campus site as the civic centre of Saanich through prominent public space(s) and a greater intensity of uses.
- B. Realize redevelopment of the Nigel Valley site.
- C. Improve connections to the Nigel Valley site, Lochside Regional Trail and Municipal Campus.
- D. Explore further enhancements to Swan Lake Nature Sanctuary.



Nigel Valley

POLICIES

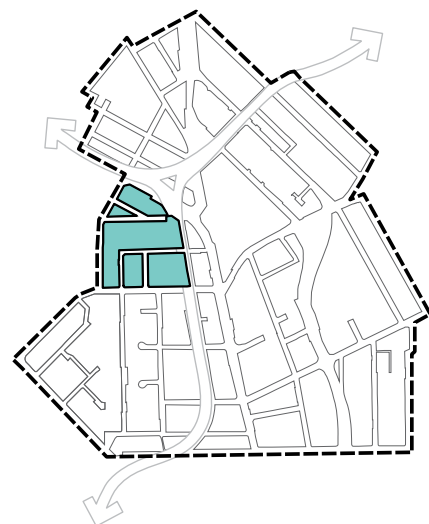
- 5.12.1 Support the addition of office, retail or housing on the Municipal Campus site as a means to better animate and connect the area.
- 5.12.2 Ensure any new development on the Municipal Campus site respects the character of the Heritage Designated Municipal Hall building through:
- Requiring the design of new buildings to respond to the height, massing and detailing of Municipal Hall;
 - Encouraging the material of new buildings to extend the language of exposed structure and dense detailing, producing shadow and interest, but not necessarily requiring a fully concrete building; and
 - Respecting heritage designated landscape features and exploring opportunities to incorporate and extend these features.
- 5.12.3 Support the introduction of a taller building on the southeast corner of the Municipal Campus site along Vernon Avenue to enhance the prominence of the site, ensuring any shadowing impacts are addressed.
- 5.12.4 Maintain a buffer between the Swan Lake Nature Sanctuary and adjacent properties and explore options to enhance landscaping (using native vegetation) in this buffer area.
- 5.12.5 Reduce the extent of surface parking on the Municipal Campus site, including through the introduction of underground parking.
- 5.12.6 Redesign the Municipal Campus site mobility network to reduce conflicts between pedestrians, cyclists and vehicles.
- 5.12.7 Support new forms of multi-unit housing between Darwin Avenue and Leslie Drive that provide a transition into the residential neighbourhood to the north (see Figure 5.1).
- 5.12.8 Support implementation of the approved Nigel Valley development concept (See also Policy 10.2.14).
- 5.12.9 Support land use changes on properties adjacent to the Nigel Valley neighbourhood that provide additional housing options, improve active transportation connections and contribute to the overall cohesiveness of the area.
- 5.12.10 Enhance the connectivity to the Lochside Regional Trail and support the creation of a public/open space at the Municipal Campus site that complements the Nigel Valley Plaza through upgrades or redevelopment (See Policy 8.3.7).
- 5.12.11 Expect parking to be accessed off of Darwin Avenue when individual redevelopment applications include parcels along both Darwin Avenue and Leslie Drive.



Looking Southeast from the Municipal Hall

5.13 TOLMIE QUARTER

The Tolmie Quarter is a unique area of the UD area whose future will largely be determined by the future use of the School Board site. A central element of any future will involve a community use on the site. Surrounding properties will incorporate a broader range of uses, providing a greater range of opportunities for business and housing suitable for a range of residents.



SUB-AREA OBJECTIVES

- A. Expand opportunities for higher density employment close to the Galloping Goose Regional Trail and School Board site.
- B. Support redevelopment on the School Board site that retains the heritage building and enhances the site's role as a community space.
- C. Support a mix of uses and housing options that enhance the area's vitality.
- D. Strengthen linkages with employment uses in Tennyson Industrial Quarter.



Looking North from Uptown Shopping Centre

POLICIES

- 5.13.1 Support the development of a master plan for the School Board site that:
- retains and revitalizes the heritage designated building;
 - incorporates community facilities and uses;
 - explores a range of uses including housing, community facilities, commercial and light industrial;
 - incorporates significant public open space; and
 - adds a pocket park, plaza or open space adjacent to the Galloping Goose Regional Trail.
- 5.13.2 Support School Board efforts should they wish to pursue re-introduction of a public school at the Boleskine Road site (See also Policy 8.4.7).
- 5.13.3 Support the expansion of light industrial use in Mixed Employment areas designated in Map 5.1 in close proximity to the School Board site provided that new development provides a high density of employment and is compatible with residential uses.
- 5.13.4 Support the exploration of a potential land swap of Regina Park lands that would create a more functional park space (See also Policy 8.2.8).
- 5.13.5 As part of the redevelopment of the properties adjoining Regina Park ensure the new development complements and supports Regina Park, including through the design of open space.
- 5.13.6 Support active commercial uses along Boleskine Road to support its role as a major pedestrian route and link to surrounding residences.
- 5.13.7 Properties adjacent to the Galloping Goose Regional Trail are expected to provide frontages that enhance the trail through design features (e.g. building design, site landscaping, pocket parks) and building massing that steps down to the trail (see also Figure 5.1, Policy 8.3.9, Guidelines 9.2.2 vi, and Map 9.2).








Tolmie School, 556 Boleskine Road



BUILDING HEIGHT DESIGNATIONS

X "X (X)" NUMBERS INDICATE BUILDING HEIGHT IN STOREYS ALLOWED UNDER "BASE" AND "(MAX)" HEIGHT CONDITIONS (SEE FIGURE 5.1 FOR ADDITIONAL DETAILS)

LAND USE DESIGNATIONS

	NEIGHBOURHOOD APARTMENT		MIXED EMPLOYMENT		PARK / TRAIL
	MID-RISE RESIDENTIAL		MIXED INSTITUTIONAL		



Historic Tolmie School and School Board Offices

6. TRANSPORTATION AND MOBILITY

Transportation routes are both the greatest strength of the Uptown–Douglas (UD) area and the biggest impediment to creating a high functioning, livable place. Major transportation routes converge in the UD area, providing a density of activity that attracts businesses and enables the efficient provision of transportation options. Douglas Street, Blanshard Street and Vernon Avenue cumulatively carry close to 100,000 vehicle trips a day, while the Galloping Goose Regional and Lochside Regional Trails are the highest volume cycling routes in the region. All of these routes run through the heart of the UD area and provide a strong basis for a complete transportation network. Transitioning these routes to be multi-modal and integrated with the public realm and new development will support the desired change from space to place.

The transition and re-design of the transportation network will require ongoing partnerships. The Ministry of Transportation and Infrastructure considers Douglas Street, Blanshard Street, Vernon Avenue and Ravine Way to be part of the highway system and has jurisdiction over these roads. The Capital Regional District is responsible for the maintenance and operation of the Galloping Goose and Lochside Regional Trails. Additionally, BC Transit's Transit Future Plan identifies Douglas Street as a Rapid Transit Corridor and highlights the development of a transit hub in the UD area as a key short-term initiative. Building a common understanding of the area's future and aligning standards and capital works initiatives will be essential to this Plan's successful implementation.

Saanich has committed to creating a mobility network that embraces different priorities than existed when the current street network was developed. The Active Transportation Plan has a target to double the proportion of trips made by walking, cycling and transit by 2036. Additionally, Saanich's new 2020 Climate Plan: 100% Renewable and Resilient Saanich commits to cutting emissions in half by 2038 and to be net zero and transition to 100% renewable energy by 2050. Given that 58% of Saanich's greenhouse gas (GHG) emissions are transportation related, major changes are needed in how infrastructure is designed and people move through and within the UD area.

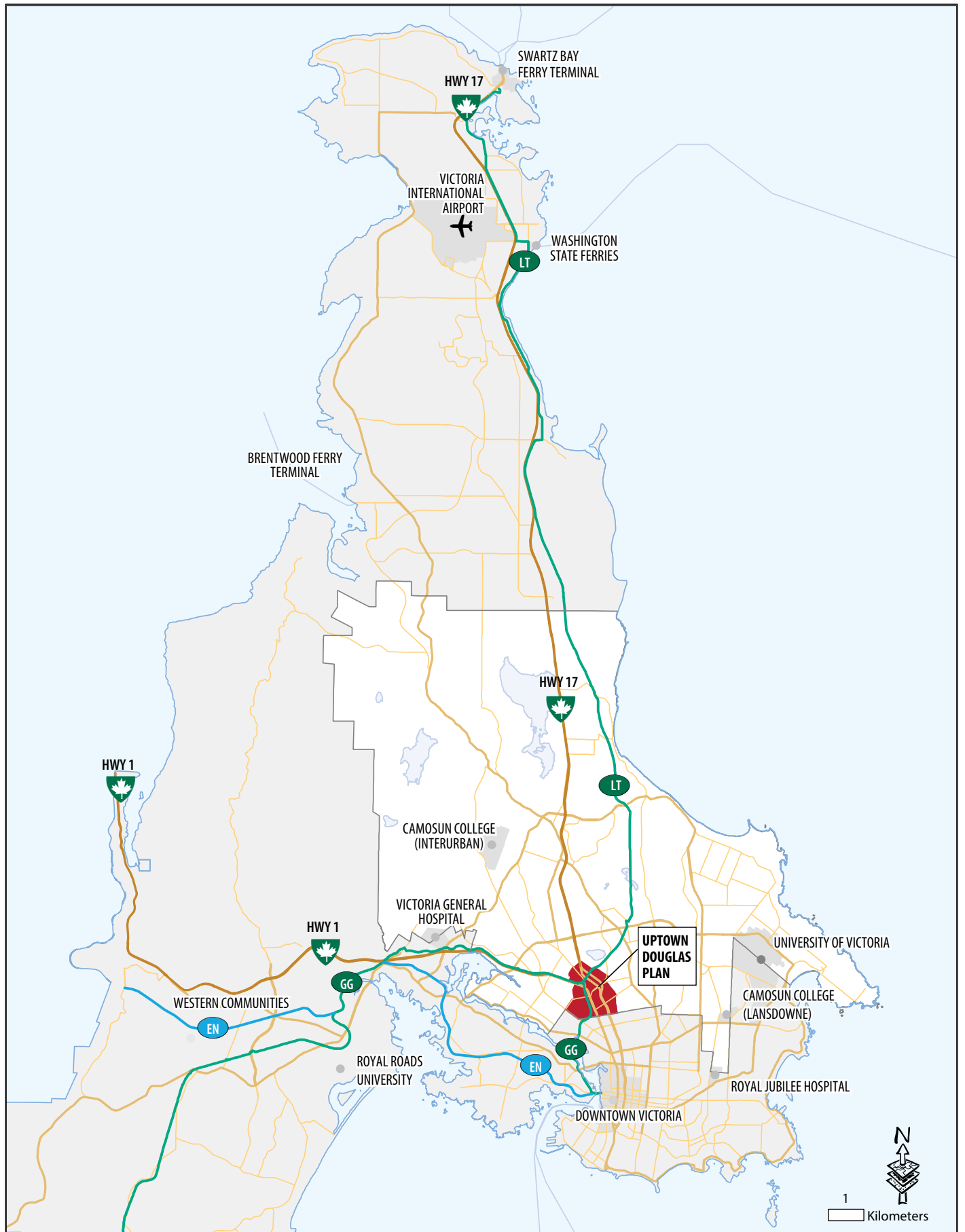
The current conditions of the UD, including its centrality in the region, high frequency transit, abundance of shops and services and high quality active transportation spines (e.g. Galloping Goose and Lochside Regional Trails), provide tangible opportunities to increase walking, cycling and transit and in turn reduce GHG emissions. The anticipated growth rates and associated land use change in the area will provide the opportunity to create complete streets, further prioritize transit, enhance goods movement and create a

complete, connected and convenient network of walking and cycling routes.

Land use and transportation planning will go hand-in-hand to reach the vision for the UD. This section provides objectives and policies to align the transportation network with the future land use described in Section 5; it further focuses on detailed aspects of the transportation system, including objectives and policies for walking, cycling, transit, vehicles, and parking.

OBJECTIVES

- A. Create a connected, sustainable and balanced transportation network that encourages more walking, cycling and transit use.
- B. Reduce carbon emissions by increasing active transportation and transit opportunities, ensuring access to electric vehicle charging and carshare programs, and creating a complete, compact and vibrant urban community.
- C. Put pedestrians first through developing an exceptional public realm and safe, convenient and attractive connections.
- D. Support infrastructure and circulation needs of businesses and balance with pedestrian, cyclist and public realm priorities.
- E. Develop complete streets through redesigning street rights-of-way to prioritize space for pedestrians, cyclists and landscaping and ensuring new development addresses the public-private interface.
- F. Create a central landmark multi-modal transit hub that includes convenient connections, a diversity of uses and public space.
- G. Enhance the Galloping Goose and Lochside Regional Trails as primary active transportation spines and build high quality connections to provide links to area destinations.
- H. Enhance connectivity for pedestrians and cyclists through breaking up superblocks, adding crossings and encouraging permeability in the development of large sites.
- I. Accelerate rapid transit implementation along Douglas Street through the application of Transit-Oriented Development (TOD) principles.



Map 6.1 Regional Transportation Context

6.1 MOBILITY NETWORK CONNECTIVITY

A connected mobility network that provides convenient and safe route options is foundational to the development of efficient multi-modal options. In the Uptown–Douglas (UD) area, major roads with limited crossing opportunities and a disconnected street network present challenges to creating

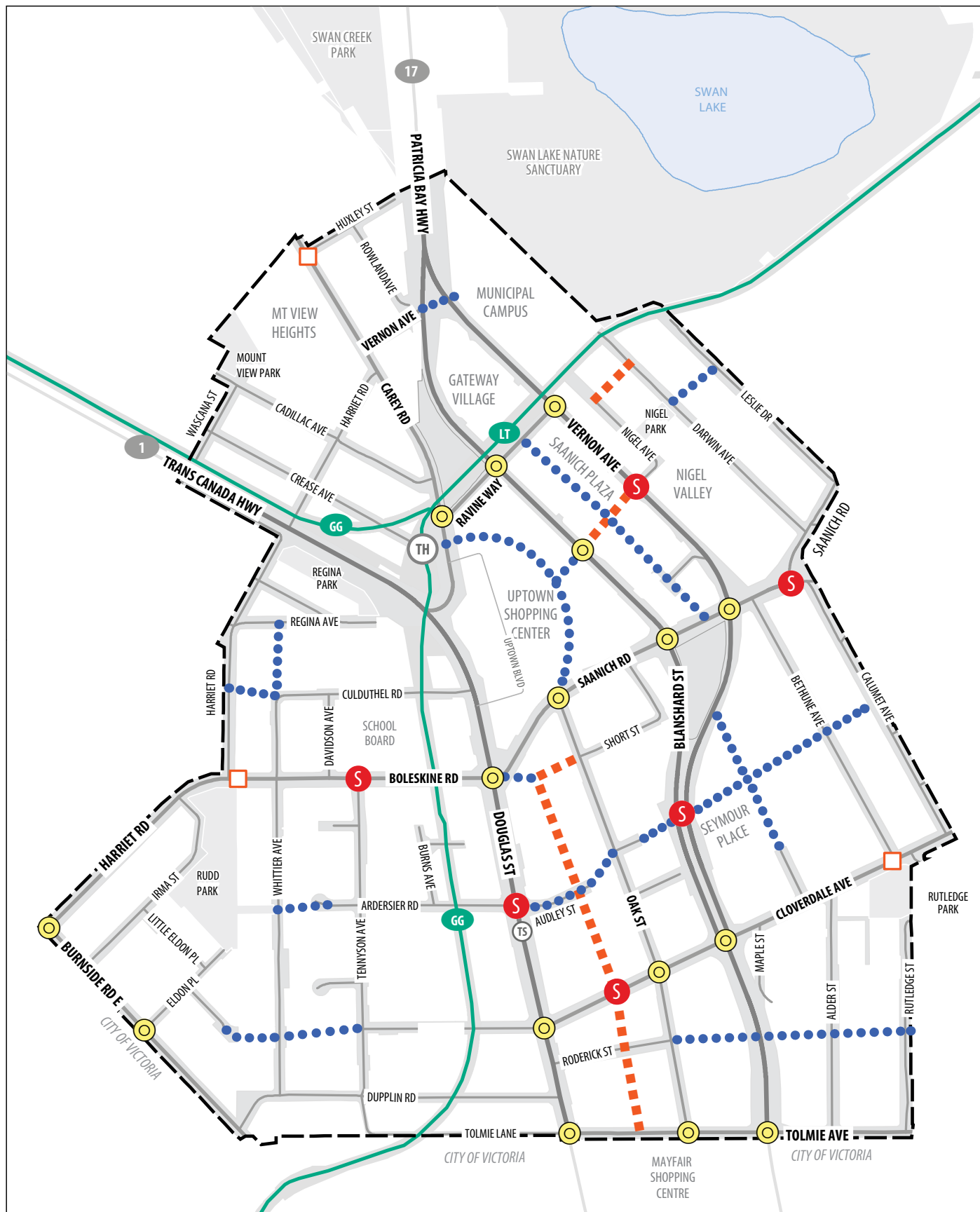
a connected network. Redesigning mobility networks to add connectivity will make walking and cycling more viable, increase the convenience of transit and improve access to businesses.



Saanich Road

POLICIES

- 6.1.1 Acquire connections for walking, cycling and goods movement, including those identified on Map 6.2, through rezoning, subdivision or property acquisition to achieve overall network connectivity.
- 6.1.2 Pursue a network of pedestrian route options including roads, trails and footpaths that provide safe and accessible choices every 100 metres, in addition to Map 6.2, to break up large blocks and enhance overall connectivity.
- 6.1.3 Improve travel across major roads through adding signals in locations identified on Map 6.2.
- 6.1.4 As part of redevelopment of the Saanich Plaza site, expect the dedication of a new municipal road to connect the driveway at Uptown Shopping Centre with Nigel Avenue and support the new road with signalization on Blanshard Street and Vernon Avenue.
- 6.1.5 Enhance pedestrian and cycling connectivity in the future to Audley Street Transit Station through creating an east-west route from Rudd Park to Calumet Avenue through a combination of new traffic signals, redesign of large sites and the introduction of pathways.
- 6.1.6 Explore the introduction of a pedestrian and cycling overpass across the Patricia Bay Highway that provides a connection to Swan Lake Nature Sanctuary from the area west of the Highway.
- 6.1.7 Expect pedestrian and cyclist mobility plans for the redevelopment of large sites (0.5 hectares and larger) and ensure that those plans address public connections to the broader neighbourhood and adjacent sites, internal site connectivity and permeability, end of trip facilities and access to building entrances.
- 6.1.8 Continue to jointly monitor and evaluate intersections and road crossing with the Galloping Goose Regional Trail and assess suitability of restrictions at intersections.
- 6.1.9 Introduce a new laneway (Audley Crossing) between Douglas Street and Oak Street to support network connectivity, access, goods movement and reduce conflicts on primary streets (See Section 7.9 and Figure 7.10).



FUTURE NETWORK

- NEW STREET / LANE
- PEDESTRIAN / BIKE CONNECTOR
- NEW TRAFFIC SIGNAL / CROSSING

EXISTING NETWORK

- TRAFFIC SIGNAL
- CONTROLLED CROSSWALK
- REGIONAL TRAIL

6.2 WALKING

Walking is the basis of mobility for people, particularly in an urban context. Every trip begins on foot and the quality of walking connections between modes or destinations is often what defines the overall quality of a mobility network. The Uptown–Douglas (UD) area poses many challenges for pedestrians, including wide major roads, sidewalks that are narrow and unseparated from traffic in many locations

and a pedestrian network that lacks connectivity and convenient route options. Creating a connected pedestrian grid, optimizing the Galloping Goose and Lochside Regional Trails, redesigning major roads and using land use to provide a more supportive pedestrian environment will help to make walking a better option, connect major destinations and support multi-modal integration.



Carey Road

POLICIES

- 6.2.1 Work towards establishing sidewalks on both sides of all streets through opportunities presented at the time of redevelopment and capital works programs.
- 6.2.2 Implement actions identified in the Active Transportation Plan including those that expand the sidewalk network and widen existing sidewalks (See Map 6.3).
- 6.2.3 Generally require a boulevard space to physically separate sidewalks from the roadway in all street improvement projects and redevelopment proposals. Exceptions may be considered if no design alternatives, such as narrowing the width of travel lanes, are possible.
- 6.2.4 Ensure the design of sidewalks and other pedestrian facilities address needs of individuals with mobility challenges and visual or auditory impairments.
- 6.2.5 Consider curb extensions as potential community spaces and explore their use for parkettes, public seating, street furniture, bike parking or public art.
- 6.2.6 Consider improvements to surface infrastructure such as sidewalks when underground projects are scheduled to optimize efficiency and minimize disruptions.
- 6.2.7 Establish an alternative transportation reserve fund (i.e. a pedestrian/cyclist infrastructure bank) for the UD area specifically for developing pedestrian and bicycle facilities with contributions based on the extent of parking variances approved.
- 6.2.8 Strongly encourage developments to provide pedestrian facilities that exceed the municipal design standards.
- 6.2.9 Explore the introduction of a pedestrian scramble design at high activity intersections as a way to enhance pedestrian priority.



PEDESTRIAN NETWORK (ATP)

- FUTURE SIDEWALK UPGRADES
- COMPLETE STREET PEDESTRIAN ENHANCEMENT
- ... NEW PEDESTRIAN / BIKE CONNECTOR
- NEW STREET / LANE
- S NEW TRAFFIC SIGNAL / CROSSING
- TRAFFIC SIGNAL
- CONTROLLED CROSS WALK

Map 6.3 Pedestrian Network

6.3 CYCLING

A network of cycling routes suited to all ages and abilities is an important component of urban mobility. Saanich's Active Transportation Plan identifies development of a cycling network that could provide facilities for those aged 8 to 80 as a priority. Saanich recognizes there are other high volume users of the Regional Trails, sidewalks and connections throughout the Uptown-Douglas (UD) area and the region. As such, this plan seeks to ensure that modes of rolling, including wheelchairs, skateboards, and scooters, are also

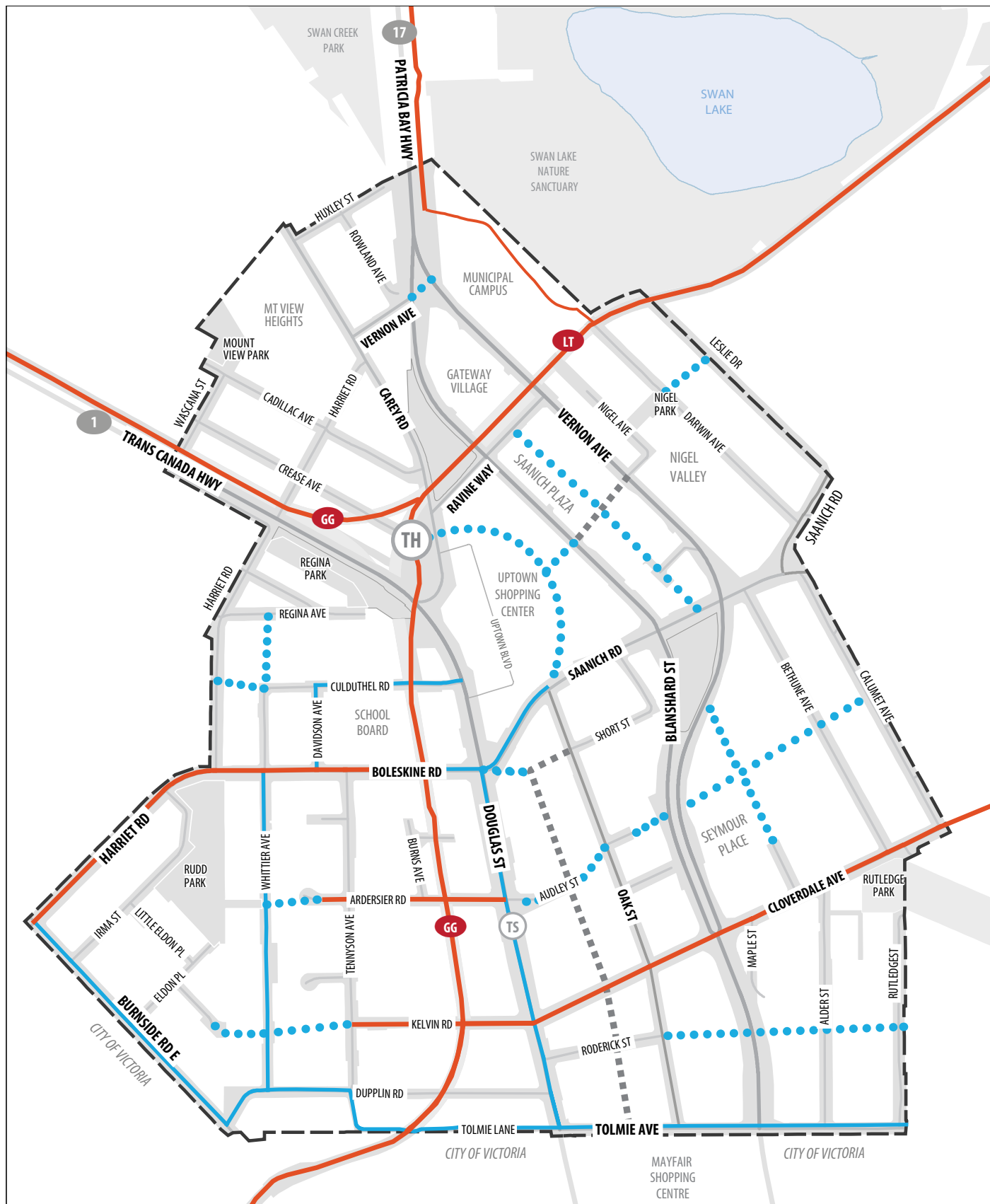
provided with a high level of service choices and routes within the mobility network. Map 6.4 illustrates the routes of this network within the UD area. While the Galloping Goose Regional and Lochside Regional Trails provide an excellent spine to build on, substantial work is required to upgrade infrastructure on other major routes. Additionally, land use changes must consider the role of individual buildings and sites as the first mile and last mile of most cycling trips and ensure they are convenient and safe.



Switchbridge

POLICIES

- 6.3.1 Implement the Active Transportation Plan's Long-Term Bicycle Network, as identified on Map 6.4.
- 6.3.2 Work towards physically separating all bike facilities on major roads from vehicle traffic, including through the use of curbs, landscaped areas and bollards.
- 6.3.3 Support developments that incorporate secure indoor and sheltered outdoor bike parking in excess of Saanich Zoning Bylaw requirements and provide facilities for e-bikes, cargo/family bikes and bike trailers.
- 6.3.4 Expect end of trip facilities be provided in new commercial developments, both for employees and customers and ensure they are secure, covered and conveniently located next to building entrances.
- 6.3.5 Expect all new developments to provide parking infrastructure to support electric bikes, including charging facilities and secure lock-up locations.
- 6.3.6 Create a separated bike facility through the Municipal Campus site to connect the Lochside Regional Trail to the Douglas Bike Connector.
- 6.3.7 Work with the CRD to include separated travel lanes for pedestrian and cyclists on the Galloping Goose and Lochside Regional Trails.
- 6.3.8 Coordinate with the City of Victoria to ensure alignment of bike corridor designs, specifically along Burnside Road East, Douglas Street and Blanshard Street.
- 6.3.9 Continue to enhance Cloverdale Avenue as a primary cycling corridor, including through the introduction of an all ages and abilities (AAA) cycling facility that is physically separated from vehicle traffic.



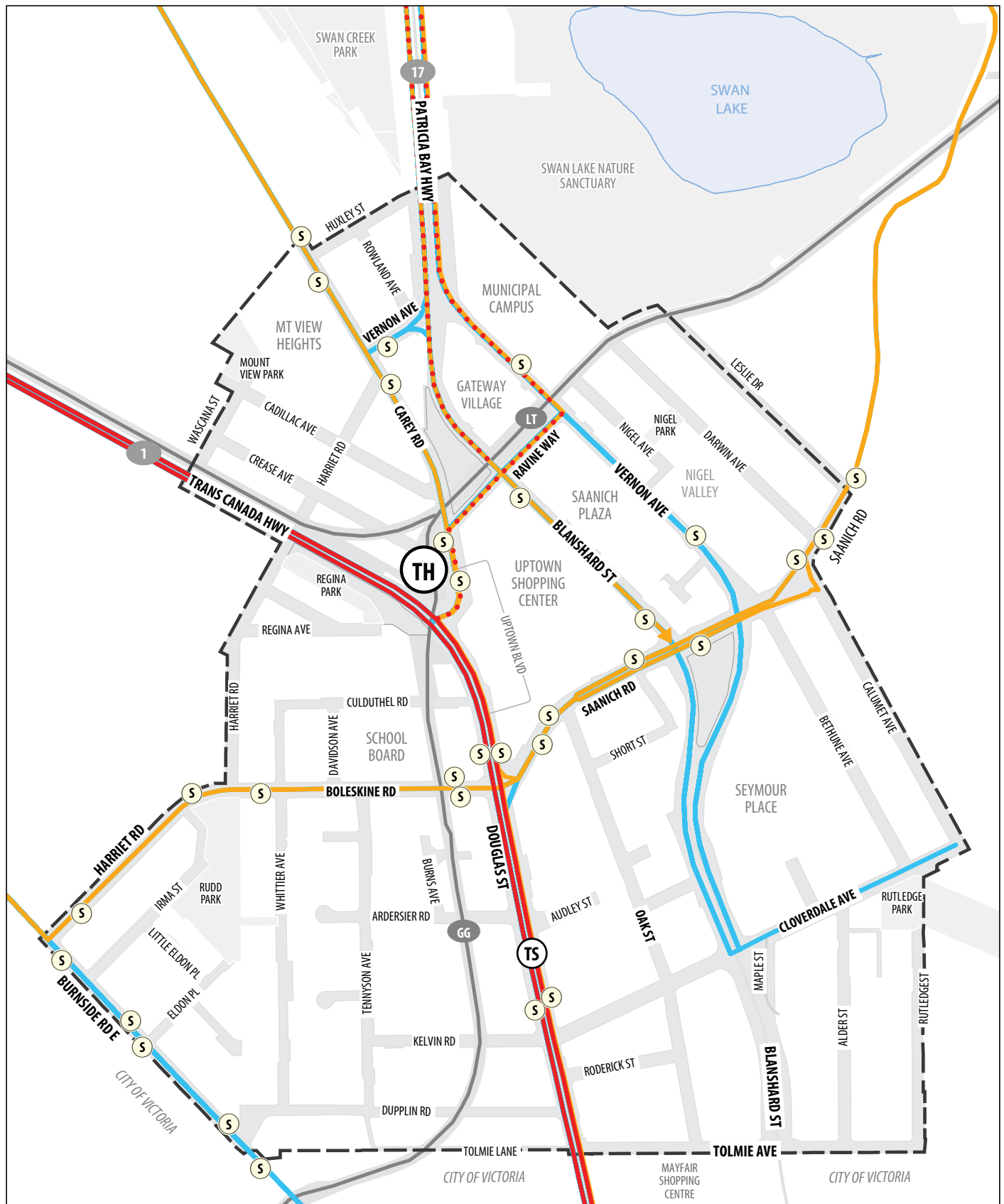
6.4 PUBLIC TRANSIT

Transit will be a primary factor in shaping the future of the Uptown-Douglas (UD) area. BC Transit's Transit Future Plan (2011) contains numerous directions that highlight the importance of the area from a transit perspective. Implementing a landmark transit hub, enhancing transit priority on Douglas Street and supporting transit-oriented development are central to creating an area that functions as the heart of Saanich. Complementary to those efforts will be

developing an urban mobility network that supports transit trips. While the area is currently well served by transit, the focus on efficiently moving buses and vehicles through the area has created a challenging environment for pedestrians and cyclists. Building complete streets and enhancing the public realm will be essential to creating an area that can truly provide high quality transit service.

POLICIES

- | | |
|-------|--|
| 6.4.1 | Support BC Transit in the implementation of the Transit Future Plan, including through improvements on routes identified on Map 6.5. |
| 6.4.2 | Support BC Transit in the creation of a landmark multi-modal transit hub at the intersection of the Galloping Goose and Lochside Regional Trails that: <ul style="list-style-type: none">• Enhances the role and identity of the UD area as a regional hub and focal point in Saanich;• Serves as a prominent visual landmark that highlights the area and serves as a point of orientation in the landscape;• Integrates high quality public spaces and services/amenities throughout the exchange; Includes attractive facades, public art, appropriate lighting and gathering spaces, including a publicly accessible rooftop open space;• Incorporates active uses, housing, retail/commercial and/or community facilities such as a community centre, library or other similar services;• Creates a seamless and inviting experience for all modes, including pedestrians, cyclists, transit users, taxis and mobility providers;• Improves the 'AAA' (all ages and abilities) quality of the Galloping Goose and Lochside Regional Trails;• Enables efficient transit access and egress along Douglas Street; and• Accommodate the future conversion to an enhanced rapid transit technology (i.e. bus rapid transit and light rail) through innovative and proactive design solutions. |
| 6.4.3 | Ensure the future multi-modal transit hub is designed with safe and convenient connections that tie into the existing pedestrian and bicycle network. |
| 6.4.4 | Provide a pedestrian connection between the multi-modal transit hub and the Uptown Shopping Centre without a street connection to provide a perceived extension of the hub. |
| 6.4.5 | Support the future introduction of the Audley Street Transit Station, including through securing open space to support station design, adding pedestrian connections to the street network and installing a traffic signal at the intersection of Douglas Street and Audley Street. |
| 6.4.6 | Collaborate with BC Transit to improve bus stop facilities including: new and upgraded shelters, street furniture, lighting, and real-time display of passenger information. |
| 6.4.7 | Support BC Transit in exploring locations for a transit staging area that can minimize the impacts of bus traffic at the Transit Hub. Ensure the staging site minimizes impacts on neighbouring properties and the aesthetic of the street. |
| 6.4.8 | Improve traffic and transit connections by providing interim priority bus lanes along Douglas Street and creating a new connection to link the Patricia Bay Highway and the future transit hub. |
| 6.4.9 | When considering implementing potential transit priority measures, ensure that quality pedestrian and cycling facilities are an integral part of the design. |



TRANSIT NETWORK

- RAPID TRANSIT NETWORK
- FREQUENT TRANSIT NETWORK
- LOCAL TRANSIT NETWORK



MULTI-MODAL
TRANSIT HUB



TRANSIT
STATION



EXISTING BUS
STOPS

6.5 MOTOR VEHICLES AND GOODS MOVEMENT

The Uptown–Douglas (UD) area plays a vital role in facilitating the movement of vehicles and goods. For many residents and businesses in the region a vehicle trip through the UD area is a regular part of their day. Maintaining safe and efficient vehicle travel and goods movement will be critical to the future of the area. Historically, the design of the area has been highly focused on vehicle movement. To realize

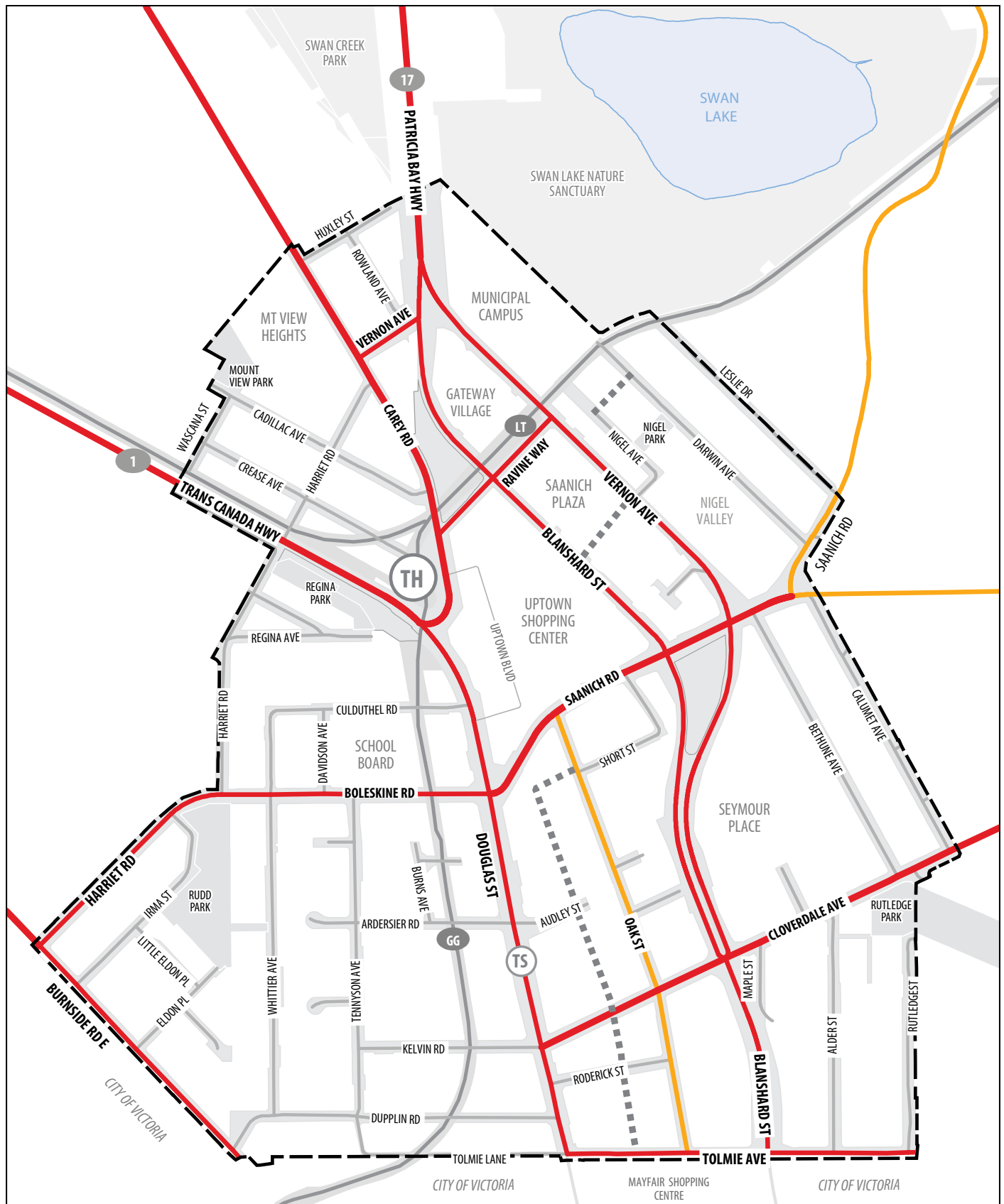
the vision of this Plan a concerted change will need to be implemented over time to provide quality facilities for pedestrians, cyclists and transit users. These changes will need to be carefully implemented to ensure they do not detract from some of the existing strategic advantages that exist in the area today.

POLICIES

- 6.5.1 Continue to design streets consistent with road designations identified on Map 6.6 and policies included in Section 7 to ensure the safe travel for all modes and efficient movement of commercial services and goods.
- 6.5.2 Include on-site loading bays in building design for commercial and industrial development (see Guideline 9.5.5 and Figure 9.7b).
- 6.5.3 Expect loading and access points to be located at the rear of the building and off of adjacent minor streets or laneways (See Section 9).
- 6.5.4 Support limited on-street loading on local roads that is identified through the redevelopment process.
- 6.5.5 As part of large lot redevelopment, pursue opportunities to improve movement and circulation, including through adding new streets or lanes, restricting access/egress to major roads and orienting building accesses to the public realm.
- 6.5.6 Encourage a more holistic transportation analysis during rezoning and subdivision application review processes that:
 - exceeds the standard assessment of traffic volumes and delays;
 - incorporates an assessment of implications for pedestrians, cyclists and individuals with mobility challenges; and
 - includes a travel plan summary (for all multi-unit residential, institutional, and commercial developments) to demonstrate strategies to minimize GHG emissions and maximize sustainable transportation modes.
- 6.5.7 Ensure industrial businesses are supported by implementing improvements that enable efficient goods movement and site access.
- 6.5.8 Promote electric vehicle charging infrastructure in all residential developments and appropriate commercial, industrial and institutional buildings to improve access for residents, employees and the public (See Policy 4.2.4).
- 6.5.9 Preserve options to integrate new technology, such as electric vehicles, autonomous vehicles, and ride sharing in the design of street rights-of-way, private parking areas, new development and redevelopment.



Douglas Street at Carey Road



ROAD CLASSIFICATION

— HIGHWAY / MAJOR
 — COLLECTOR
 — RESIDENTIAL
 - - - - NEW ROAD / LANE

6.6 PARKING

A significant portion of the land area in the Uptown–Douglas (UD) area is currently dedicated to surface parking and the provision of parking has historically been a primary determinant of site design. While many businesses and residents will continue to rely on an adequate supply of parking, the demand is anticipated to diminish as travel options to access daily needs become more plentiful and convenient. As part of a transition to a more complete community, parking will become a less prominent component of the landscape. This transition will involve shifting parking underground and introducing transit, cycling and walking facilities that will help reduce the proportion of vehicle trips, thus reducing the amount of parking required.

A fundamental change identified by this Plan is the alignment of off-street parking standards to reflect the multitude of travel options and more accurately match the parking demand that is experienced in this denser urban area of Saanich. This change will help to increase the viability of new development and reduce the amount of space that is dedicated to vehicle storage.

The management of on-street parking in the UD area will also change over the next 30 years. Currently, there is no pay parking and limited time restricted parking in the area, both of which can help to manage parking demand. As the intensity of use increases, there will be a heightened need to manage parking in a way that maximizes efficiency, supports business operations and limits impacts on residents.

POLICIES

- 6.6.1 Require parking to be located underground, or in unique circumstances, under building.
- 6.6.2 Notwithstanding Policy 6.6.1, permit limited surface parking, for disabled and visitor parking and industrial uses, if located at the rear of buildings and landscaped and/or screened from public view (see Guidelines 9.2.5).
- 6.6.3 Encourage parking areas to be broken up with pedestrian connections that provide safe and legible access to building entrances and between sites.
- 6.6.4 Following the adoption of this Plan, review and update parking requirements in the Zoning Bylaw with the intent to lower requirements in the Uptown-Douglas area to reflect:
 - The central location of the UD and proximate access to goods and services;
 - Access to frequent transit service and availability of high quality cycling and walking facilities;
 - Existing and projected parking demand in the area; and
 - The range of uses and housing types supported through land use designations.
- 6.6.5 Acknowledge the central location and availability of alternative transportation options in the UD area by considering parking variances for commercial, institutional and residential uses. Assess the extent of the potential variance based on the following factors:
 - Pedestrian and cycling network improvements (including the addition of new pathways);
 - Location relative to major transit stations (is within 250 metre / 3-minute walking distance);
 - Integration of major transit stops into the development;
 - Location relative to the Galloping Goose Regional or Lochside Regional Trail (is within 250 metre/ 3 minute walking distance);
 - Provision of non-market housing, rental housing and smaller unit sizes (for residential developments);
 - Provision of Class A bike parking spaces in excess of the Zoning Bylaw requirements;
 - Design and orientation of building(s) clearly prioritizes access for pedestrians, cyclists and transit users;
 - Provision of car share vehicles, parking spots and/or memberships; and
 - Other elements that support alternative transportation modes and reduce reliance on motor vehicles.
- 6.6.6 Consider opportunities for shared parking in mixed-use developments.
- 6.6.7 Prepare an inventory of existing parking restrictions and utilization rates and use information to develop a Uptown-Douglas parking management strategy.

- 6.6.8 Continue to explore on-street parking in commercial and residential areas to accommodate and prioritize short-term needs including through loading, time-limited, metered and residential only parking zones.
- 6.6.9 Support the provision of designated on-street car-share spaces in the Uptown-Douglas area.
- 6.6.10 Balance on-street parking needs of local residents, businesses and consumers with appropriate resources and tools (e.g. time-limited, metered and residential-only parking zones).
- 6.6.11 Provide flexibility in parking requirements for commercial uses where a change in use occurs in an existing building.
- 6.6.12 Support parking above the second storey of new developments provided the structure is designed:
- with quality screening through architectural detail on the facade of the building; and
 - to enable adaptive reuse/flexibility of use of the floors in the future (i.e. future commercial units).



Underground parking off Huxley Street

7. SIGNIFICANT STREETS

The purpose of this section is to profile and provide direction for streets that will be particularly significant to how the Uptown-Douglas (UD) area will grow and change into the future. These streets include major roads, but also smaller scale streets that will be integral to the success of the area.

The long term vision is to create a network of complete streets that are designed to ensure users of all abilities, including pedestrians, cyclists, transit users and drivers, feel safe and comfortable. The feel, function and character of the UD area is strongly defined by its major roads, and to realize the Vision of the area, these roads will need to transition into complete streets, each in a way that complements the surrounding land use context.

The treatment of Douglas Street, Blanshard Street and Vernon Avenue as highways has created a design that is inconsistent with the context of a dense urban area. Transitioning these streets to context-sensitive urban arterials could perhaps be the single most important step in enhancing the livability of the area. As the streets are currently under the jurisdiction of the Ministry of Transportation and Infrastructure (MOTI), a collaborative approach will be needed to ensure the vision for the area can be translated to design solutions on these major roads.

The current treatment of Saanich Road and Boleskine Road are the product of several challenges, including high traffic volumes over extended peak hours, numerous controlled stops, crossings that span over three MOTI highways and the operation of frequent transit (bus) routes.

In a different sense, the future design of Oak Street will also have a significant impact on the character of the area. The future vision of the area as a medium density residential high street without a major role in carrying high traffic volumes provides a unique opportunity to create an innovative design with a special character. The addition of Audley Crossing between Douglas Street and Oak Street will enable many access and loading functions to be removed from Oak Street, provide additional space for commercial activity and introduce a finer grained pedestrian network.

This section provides a holistic vision for a future design of several streets in the UD area including standard cross-sections, namely: Douglas Street, Blanshard Street, Vernon Avenue, Ravine Way, Saanich Road, Boleskine Road/Harriet Road (south), Oak Street, and Audley Crossing. Objectives and policies seek to transform these significant streets and signify their role and importance.

OBJECTIVES

- A. Reduce the dominance of motor vehicles and associated impacts on safety and quality of place.
- B. Transform major and collector roads into complete streets that provide high quality facilities for pedestrians, cyclists and transit users.
- C. Frame streets with pedestrian-oriented building design.
- D. Support public realm improvements that emphasize the special character and identity of each street.
- E. Phase out highway-like features of the major roads, including slip lanes and wide travel lanes.
- F. Ensure new transit stops are high quality and consider the progression toward more frequent transit service.
- G. Implement a high standard of cycling facilities suitable for all ages and abilities, prioritizing adequate space and protection for cyclists.
- H. Provide adequate space in boulevards for landscaping, furniture, and other elements that improve the pedestrian experience and safety.



Douglas Street southbound, at Ardersier Road.

7.1 GENERAL DESIGN OF STREETS

A key emphasis of this section is developing a new philosophy towards the design of major streets in the Uptown–Douglas (UD) area. Assessing any potential design changes both within the rights-of-way and to adjacent land uses will require careful consideration of potential impacts to all modes and all ages and abilities. By designing for the

most vulnerable members of society, we are able to create a comfortable street environment for everyone. Key to these changes will be working in collaboration with the Ministry of Transportation and Infrastructure (MOTI), who have jurisdiction over many major road rights-of-way.

POLICIES

- 7.1.1 Design all major and collector roads in the UD area to be complete streets that comfortably accommodate walking, cycling, transit and vehicle travel for all ages and abilities.
- 7.1.2 Continue to work with the MOTI to implement the right-of-way designs on Ministry roads that are complete streets and consistent with the UD's urban land use context.
- 7.1.3 Develop a Memorandum of Understanding (MOU) with the MOTI which identifies agreed upon design and development approaches that reflect the UD context and policy directions and provides predictability for future capital projects and improvements required as part of re-development proposals.
- 7.1.4 Explore options for Saanich to gain control of MOTI owned roads for the purpose of implementing complete street designs on urban arterial roads.
- 7.1.5 Work with MOTI, either through an MOU or as part of individual proposals, to reduce the required 4.5 metre setbacks from property lines on Ministry roads in order to create a pedestrian-oriented public realm with buildings close to the street.
- 7.1.6 Enhance pedestrian conditions at intersections by:
 - Minimizing pedestrian crossing distances;
 - Removing right turn islands;
 - Reducing corner turn radii;
 - Providing sufficient waiting space at corners;
 - Improving visibility of pedestrians; and
 - Implementing designs that consider individuals with mobility, auditory, and visual challenges.



Source: Tourism Victoria

Whole Foods, Blanshard Street

7.2 DOUGLAS STREET

Douglas Street is a major corridor connecting the Greater Victoria core to the Westshore via Trans-Canada Highway (Highway 1). With approximately 40,000 vehicles passing

through daily, the street feels and functions as a highway. In future, Douglas Street will be more multi-modal, with street oriented uses and clear transit priority.

POLICIES

- 7.2.1 Within existing right-of-way, implement the conceptual design for Douglas Street identified in Figure 7.1.
- 7.2.2 Pursue options to reduce lane widths on Douglas Street to dimensions consistent with accepted standards for urban arterial streets, as follows:
 - 3.0 to 3.1 metres for travel lanes;
 - 3.3 metres for turn lanes; and
 - 3.5 metres for bus lanes.
- 7.2.3 Acquire additional right-of-way on Douglas Street, as redevelopment occurs, to achieve a minimum:
 - 35 metre right of way between Tolmie Avenue and Ardersier Road / Audley Street where no centre turn lane exists;
 - 38 metre right of way between Tolmie Avenue and Ardersier Road / Audley Street where a centre turn lane exists; and
 - 40 metre right of way between Ardersier Road / Audley Street and Carey Road.
- 7.2.4 Within the future 35-40 metre right of way on Douglas Street, implement the conceptual design in Figure 7.2 and accommodate the following features on both sides of the street:
 - Minimum 2.5 metre sidewalk;
 - Minimum 2.0 metre cycle track with 0.5 metre buffer;
 - Minimum 2.5 metre boulevard; and
 - Dedicated right-of-way space for transit and high quality transit stops.
- 7.2.5 Ensure any changes or frontage improvements on Douglas Street align with future design concepts, including ensuring trees are planted in locations where they will not be removed when the future expanded design is implemented.
- 7.2.6 Support the development of high quality transit stops on Douglas Street by requiring additional right-of-way, supporting designs that integrate transit stops into buildings and limiting landscaping that could impact pedestrian circulation.
- 7.2.7 Work with property owners to achieve an effective pedestrian realm that extends beyond the property line and utilizes private property space for weather protection, seating, lighting and street furniture.
- 7.2.8 Support capital projects that work towards enhanced transit service and the introduction of rail on Douglas Street.
- 7.2.9 Ensure sidewalks are separated from vehicle traffic by boulevards, bike lanes or utility strips.
- 7.2.10 Define a four to six storey street wall along Douglas Street to frame the street (See Figures 7.1 and 7.2).
- 7.2.11 Design street corners with additional tree/landscaping plantings and benches to create interest and community interaction.
- 7.2.12 Ensure all new developments have zero or minimal setbacks on Douglas Street to create a pattern of continuous commercial frontages.
- 7.2.13 Orient building entrances to Douglas Street and ensure storefronts are transparent and invite activity and interaction.
- 7.2.14 Design Douglas Street as a major commercial and transit corridor, with active uses at street level, wide sidewalks and branding elements that highlight its role as a rapid transit corridor.

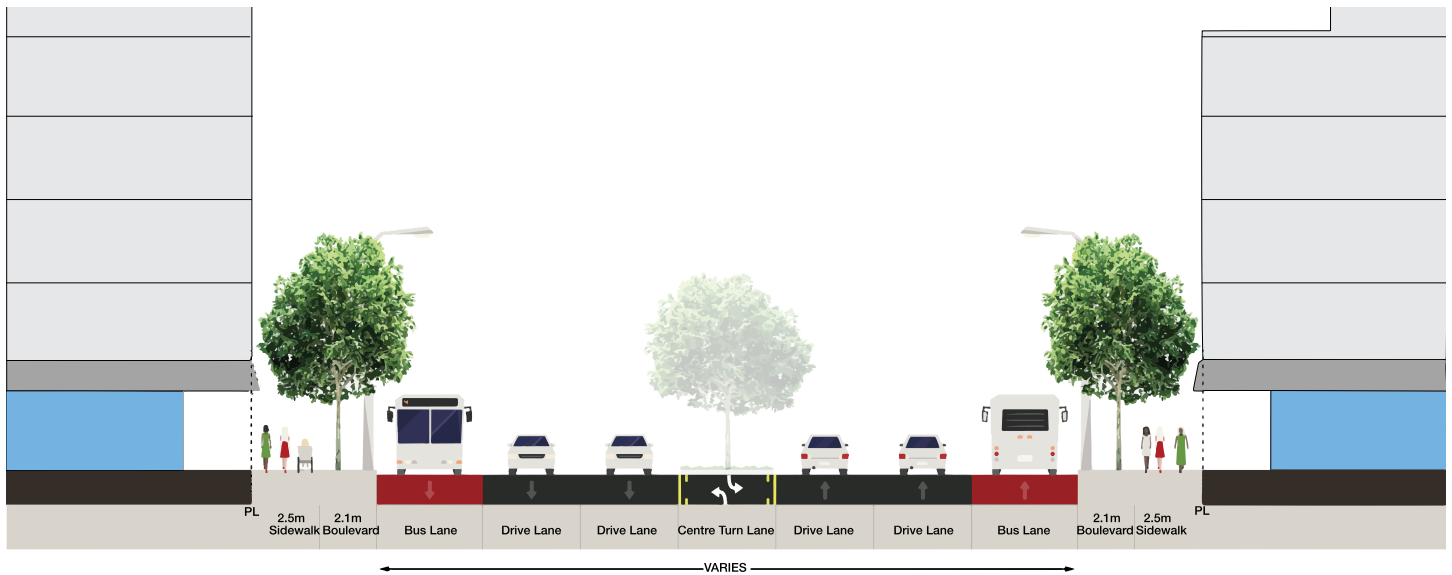


Figure 7.1: Conceptual cross section for Douglas Street within existing Right of Way (variable width)

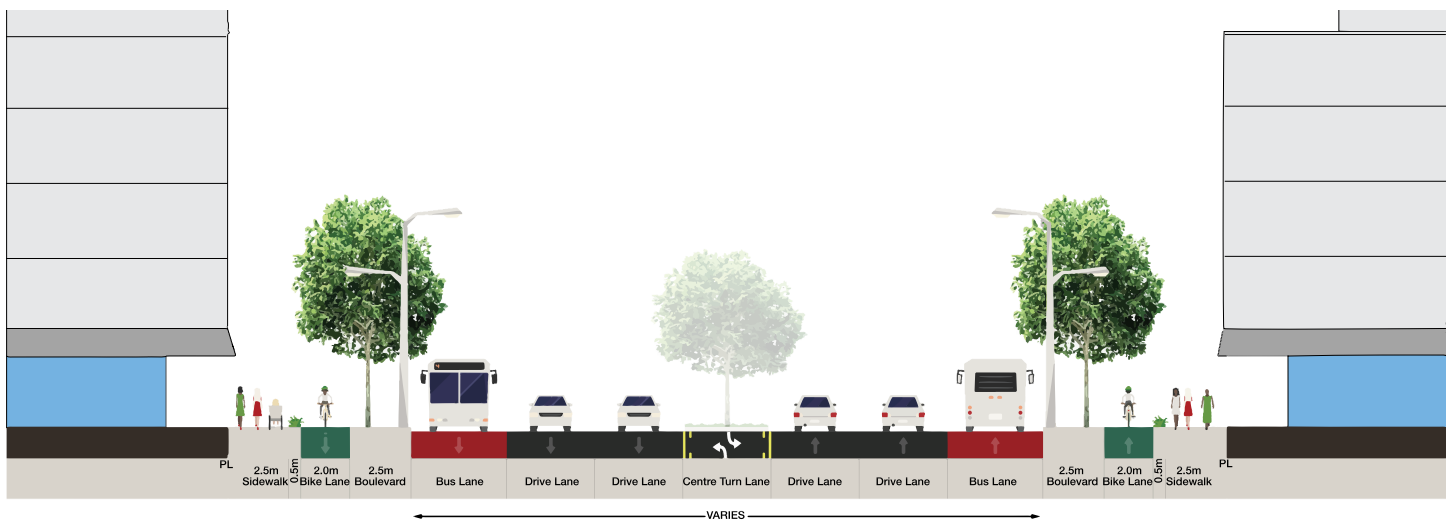


Figure 7.2: Long-term conceptual cross section for Douglas Street within an expanded Right of Way (generally 35-40 metres)

7.3 BLANSHARD STREET

Blanshard Street is a major corridor that has a primary role in carrying high vehicle traffic volumes. It forms a couplet with Vernon Avenue at its juncture with the Patricia Bay Highway and transitions to a six-lane road that continues into the City of Victoria. The southern portion contains a wide treed median,

narrow, unseparated sidewalks, narrow bike lanes and land uses that are largely disconnected from the street. As the Uptown–Douglas (UD) area evolves, Blanshard Street will transition to a complete street that supports walkability and connects buildings to the street.



Looking Southeast towards Seymour Place

POLICIES

- 7.3.1 Acquire additional right-of-way, as redevelopment occurs, from properties on the north side of Blanshard Street between Vernon Avenue and Saanich Road to achieve a minimum 22 metre right-of-way on the one-way section of Blanshard Street.
- 7.3.2 Work with the Ministry of Transportation and Infrastructure (MOTI) to analyze the feasibility of reducing the one-way section of Blanshard Street to three lanes, similar to the reduction being undertaken on Vernon Avenue.
- 7.3.3 Implement the conceptual design identified in Figure 7.3 on one-way sections of Blanshard Street, subject to the results of the analysis identified in Policy 7.3.2.
- 7.3.4 Implement the conceptual design identified in Figure 7.4 on two-way sections of Blanshard Street
- 7.3.5 Where feasible, reduce intersection crossing distances, increase signal crossing times and introduce median/green refuge islands to support two-stage crossings.
- 7.3.6 Work with MOTI to plant large canopy trees and introduce public art in the Blanshard Street median.

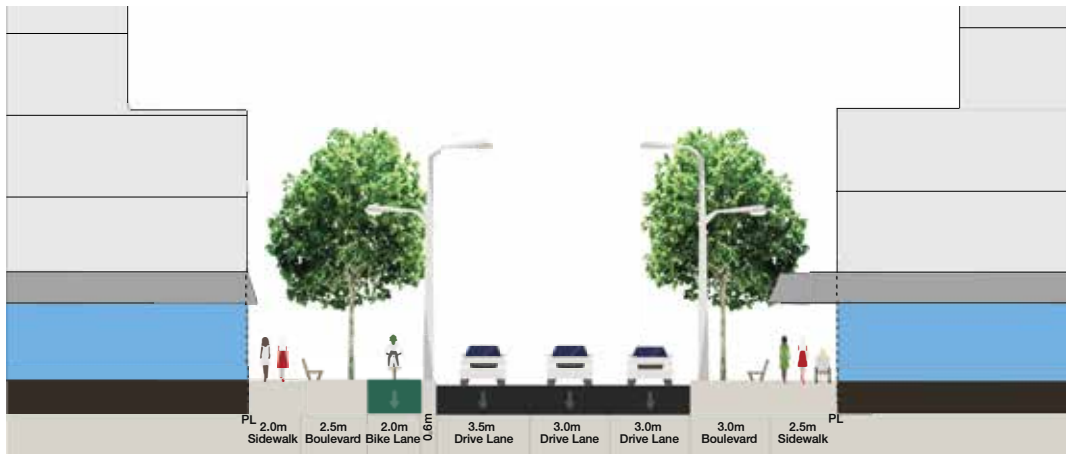


Figure 7.3: Conceptual cross section for Blanshard Street – One way (22 metres)

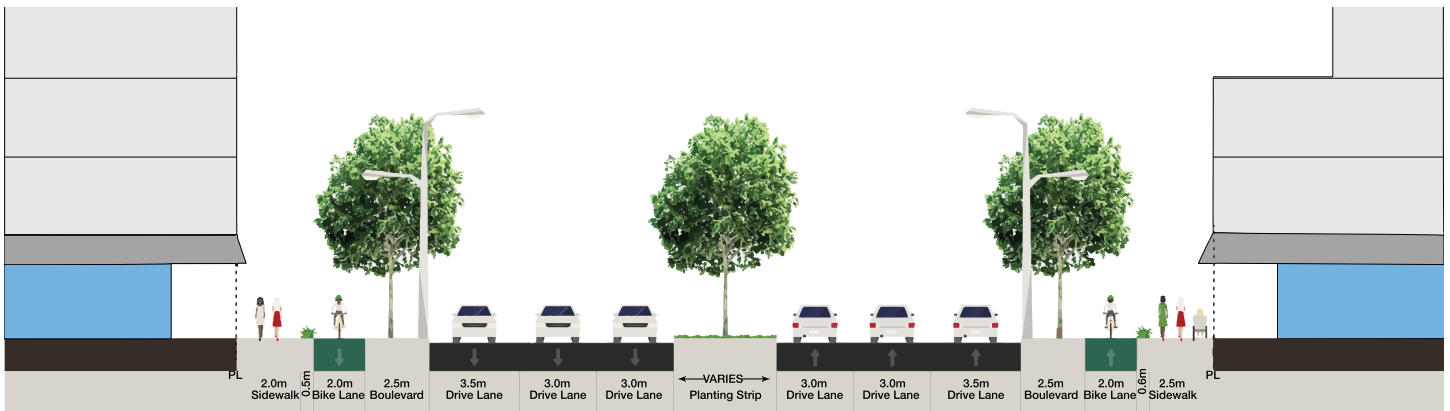


Figure 7.4: Conceptual cross section for Blanshard Street – Two-way (variable width)

7.4 VERNON AVENUE

Vernon Avenue is a one-way street that is approximately one kilometre in length and forms one half of the couplet that transitions from the Patricia Bay Highway to the urban core of Saanich and the two-way segment of Blanshard Street. This road is short, but connects some important landmarks in the Uptown–Douglas (UD) area, including the Municipal Campus, Saanich Plaza, Lochside Regional Trail and Nigel Valley. The road was originally designed as a highway and that is reflected in the narrow sidewalks and bike lanes, lack of landscaping, high number of vehicle lanes and auto-

oriented land uses. Major sites along Vernon Avenue will redevelop over the time frame of this Plan, starting with the Nigel Valley development. This provides an opportunity to redesign Vernon Avenue as a street that connects, rather than divides the community. From a design perspective, this includes a complete street design that meaningfully accommodates pedestrians, cyclists, landscaping and vehicles, as well as street-oriented land uses that help to reinforce a vibrant and active public realm.

POLICIES

- 7.4.1 Where necessary, acquire additional right-of-way, as redevelopment occurs, from properties on the south side of Vernon Avenue to achieve a minimum 22 metre right-of-way on Vernon Avenue.
- 7.4.2 Implement the conceptual design identified in Figure 7.5 on Vernon Avenue.
- 7.4.3 Work towards the removal of deceleration lanes on Vernon Avenue.
- 7.4.4 Orient new development towards Vernon Avenue including through entrances that connect to the pedestrian realm.
- 7.4.5 Work with the Ministry of Transportation and Infrastructure (MOTI) to add an additional pedestrian crossing on Vernon Avenue between Saanich Road and Ravine Way.
- 7.4.6 Explore options to better integrate and connect the Nigel Valley site and Municipal Campus site to the rest of the UD area, including through streetscape improvements on Vernon Avenue and active transportation connections.

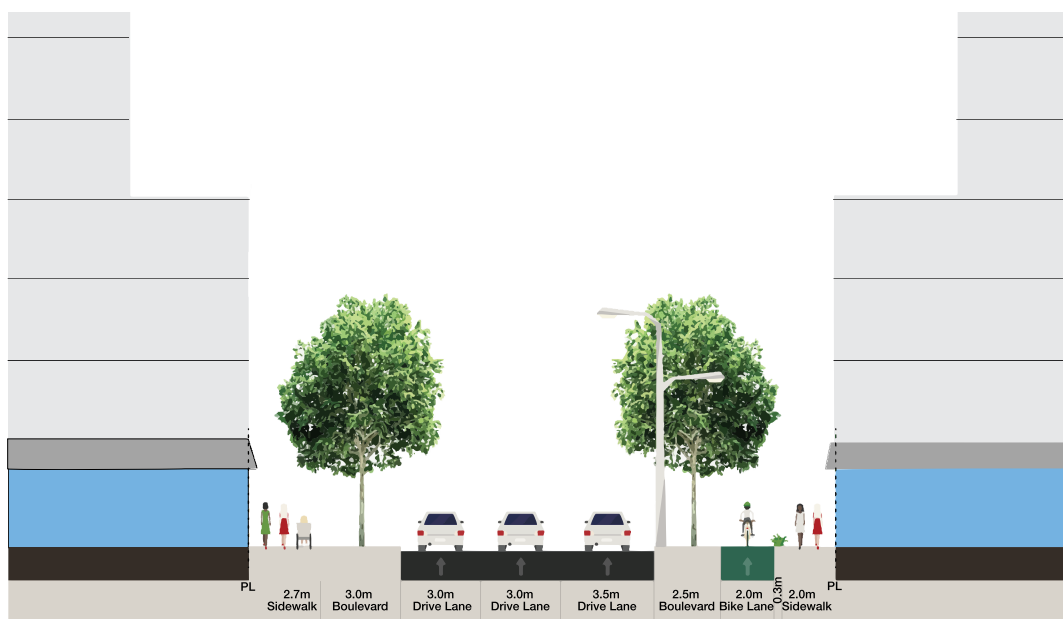


Figure 7.5: Conceptual cross section for Vernon Avenue (22 metres)

7.5 RAVINE WAY

Portions of Ravine Way have begun to transition toward a complete street through the Uptown Shopping Centre development.

The continued transition of Ravine Way to a complete street will include the development of an inviting and connected pedestrian realm with access to adjacent commercial and residential uses, a two way bike lane and a two way road way. In future, it is also envisioned that access to the Lochside Regional Trail is improved and an additional connection point is explored.

POLICIES

- 7.5.1 Implement the conceptual design identified in Figure 7.6 on Ravine Way.
- 7.5.2 Where possible, add a sidewalk facility at street level on the west side of Ravine Way, either within the road right-of-way or within the Lochside Regional Trail right-of-way.
- 7.5.3 Explore adding a new connection to the Lochside Regional Trail from Ravine Way.
- 7.5.4 Explore opportunities to enhance transit priority on Ravine Way.

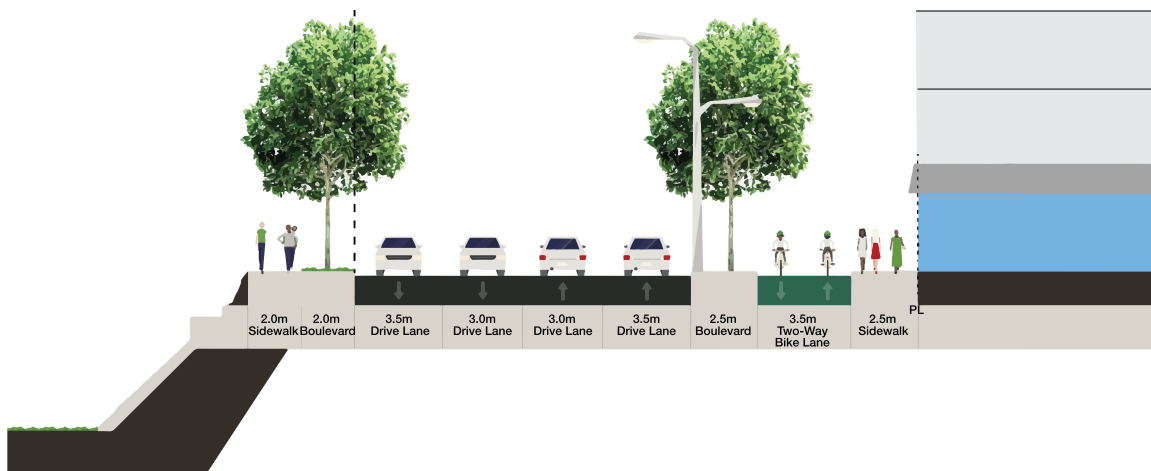


Figure 7.6: Conceptual cross section for Ravine Way (20 metres)

7.6 SAANICH ROAD

Saanich Road is an important connection within Saanich and the Uptown-Douglas Corridor. Classified as a major road, Saanich Road combines with Boleskine and Harriet Roads to form an east-west connection through Saanich. Saanich Road is a transit corridor and functions as an important link for all travel modes. The section of Saanich Road between Douglas Street and Tattersall Road is subject to particularly high traffic volumes during peak times. As congestion is a regular occurrence in this section, the District of Saanich continues to implement improvements and work with the

Ministry of Transportation and Infrastructure (MOTI) with the goal of improving movement for all users.

Saanich Road will remain an important commuter and circulation route and will connect key community destinations by foot, bike, transit and motor vehicle. It will be designed to welcome an influx of people that will live, work and recreate in the area, connecting a pedestrian oriented Oak Street and Uptown Shopping Centre.

POLICIES

- 7.6.1 Where necessary, acquire additional right-of-way, as redevelopment occurs, from properties along Saanich Road between Douglas Street and the intersection of Tattersall Road, to achieve the conceptual design identified in Figure 7.7.
- 7.6.2 Implement the conceptual design identified in Figure 7.7 on Saanich Road.
- 7.6.3 Accommodate additional turn and/or auxiliary lanes at major intersections when required.
- 7.6.4 Continue to work with the Ministry of Transportation and Infrastructure (MOTI) to improve the safety and movement of all modes and the quality of the public realm along Saanich Road.
- 7.6.5 Support the development of high-quality transit stops on Saanich Road by requiring additional right-of-way, supporting designs that integrate transit stops into buildings and provide landscaping at the edges of pedestrian circulation areas.
- 7.6.6 Explore opportunities to enhance the pedestrian and vehicular connection between Oak Street and Uptown Shopping Centre, including through prominent planting, lighting and/or public art.
- 7.6.7 Consider intersection safety when implementing the conceptual design for Saanich Road and other streets which it connects with or crosses.
- 7.6.8 Install a traffic signal at the intersection of Saanich Road and Tattersall Road which includes a left turn signal from Saanich Road onto Tattersall Road, as per the Active Transportation Plan.

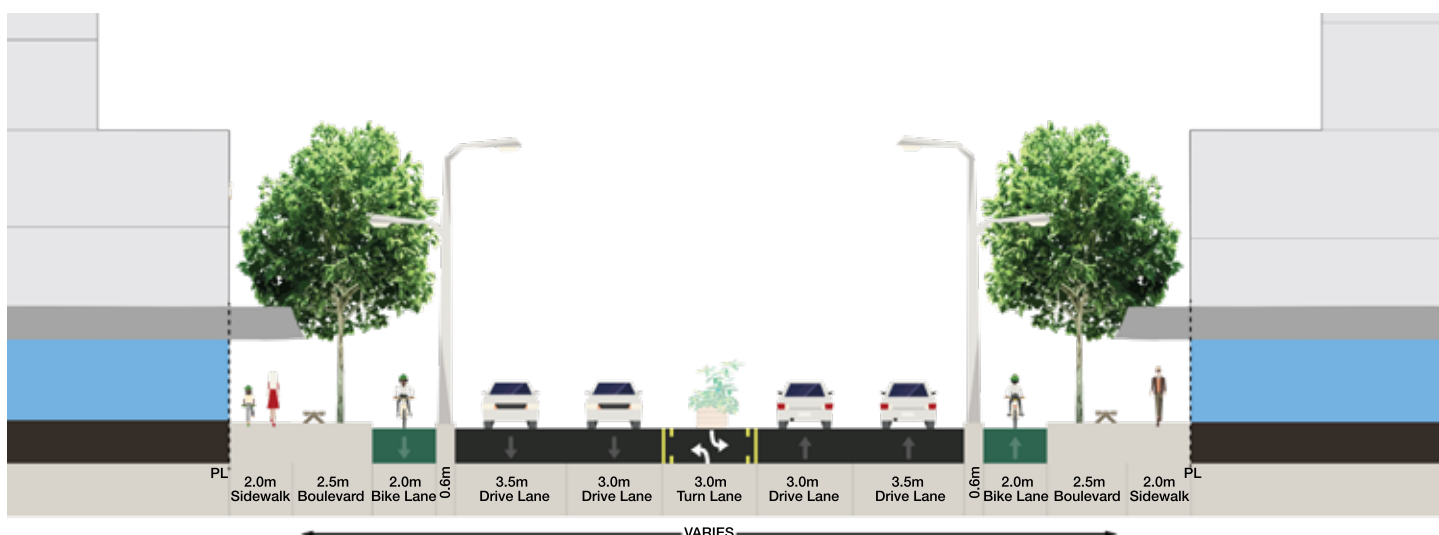


Figure 7.7: Conceptual cross section for Saanich Road (width varies)

7.7 BOLESKINE ROAD/HARRIET ROAD (SOUTH)

Boleskine Road and Harriet Road (south) provide an important western link directly through the center of the plan area, via a continuation of Saanich Road west of Douglas Street. This route has seen some recent changes, including the addition of a new pedestrian crossing at the north end of Rudd Park and Harriet Road.

The neighbourhood surrounding this corridor is likely to see a moderate shift in land use. Properties along Boleskine Road will continue to be a mix of commercial, light industrial

and residential but with increases in density. Properties along Harriet Road will transform from primarily single detached residential to a low-medium density townhomes, stacked townhomes and apartments. This added density, along with planned capital expansion and improvements to Rudd Park, requires a complete design of the right-of-way that is safe, accessible and comfortable, and that prioritizes the needs of the adjacent residents of the neighbourhood.

POLICIES

- 7.7.1 Implement the conceptual design identified in Figure 7.8 on Boleskine Road and Harriet Road.
- 7.7.2 Install a new traffic signal at the intersection of Boleskine Road and Tennyson Avenue.
- 7.7.3 Support commercial and mixed-use developments that improve the streetscape design of Boleskine Road.
- 7.7.4 Work with the City of Victoria to ensure consistency in design at the intersection of Harriet Road and Burnside Road East.
- 7.7.5 Explore opportunities to enhance the pedestrian connection between Boleskine Road /Harriet Road (south) and Rudd Park, including through signage, park access points, prominent plantings, lighting and/or public art.

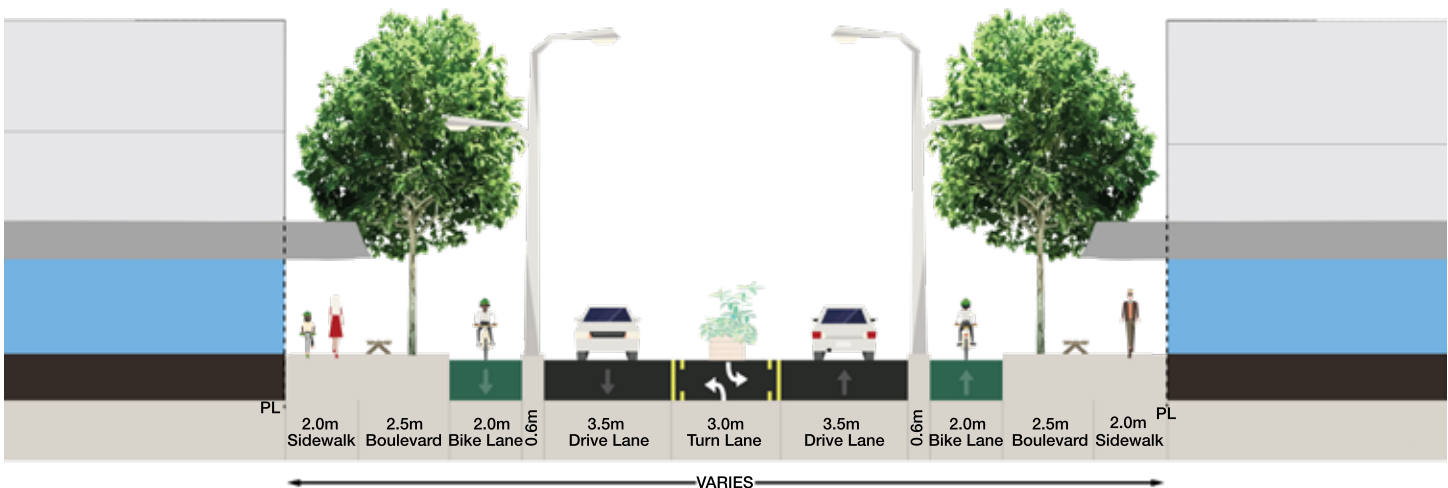


Figure 7.8: Conceptual cross section for Boleskine Road (width varies)

7.8 OAK STREET

Oak Street is one of the few significant streets in the Uptown–Douglas (UD) area that is not heavily impacted by high traffic volumes. It is currently lined by almost exclusively commercial businesses, including a number of car dealerships. Oak Street is situated between Douglas Street and Blanshard Street with direct links to Uptown Shopping Centre and Mayfair Mall, it provides unique opportunities for its future design and role in the UD area.

Oak Street will be designed as a central high street, integrating bikes, a pedestrian promenade and amenities with active and engaging mixed-use buildings framing the street. Livability will be the primary consideration in the design of the street and the buildings fronting it.

POLICIES

- 7.8.1 Implement the conceptual design identified in Figure 7.9 on Oak Street.
- 7.8.2 Design Oak Street as a featured pedestrian street with street trees and street furniture (including benches, pedestrian lighting, movable seating, and resting places).
- 7.8.3 Support developments that improve streetscape design of Oak Street that serves to enhance its role, function and appearance as a pedestrian-oriented main street.
- 7.8.4 Define a three-storey street wall along Oak Street to frame the street and reflects its smaller scale character (See Figure 7.9).
- 7.8.5 Support changes on Oak Street that increase animation and pedestrian orientation, including the addition of parks, plazas, patios and active uses.
- 7.8.6 Support the installation of parklettes along Oak Street that use parking spaces to create temporary pocket parks.
- 7.8.7 In collaboration with the business community, support temporary closures of Oak Street for festivals or other community celebrations.
- 7.8.8 Strengthen the pedestrian connections to the existing Uptown Boulevard where Oak Street and Saanich Road intersect, including through paving, wayfinding and intersection design.
- 7.8.9 Restrict access points off of Oak Street, locating access on side streets or from Audley Crossing, where possible.
- 7.8.10 Provide street lighting with separate light sources for pedestrians and vehicles.
- 7.8.11 Explore opportunities to plant Garry Oak trees in park spaces or feature areas along Oak Street to celebrate the Street's name and the area's natural history.



Oak Street

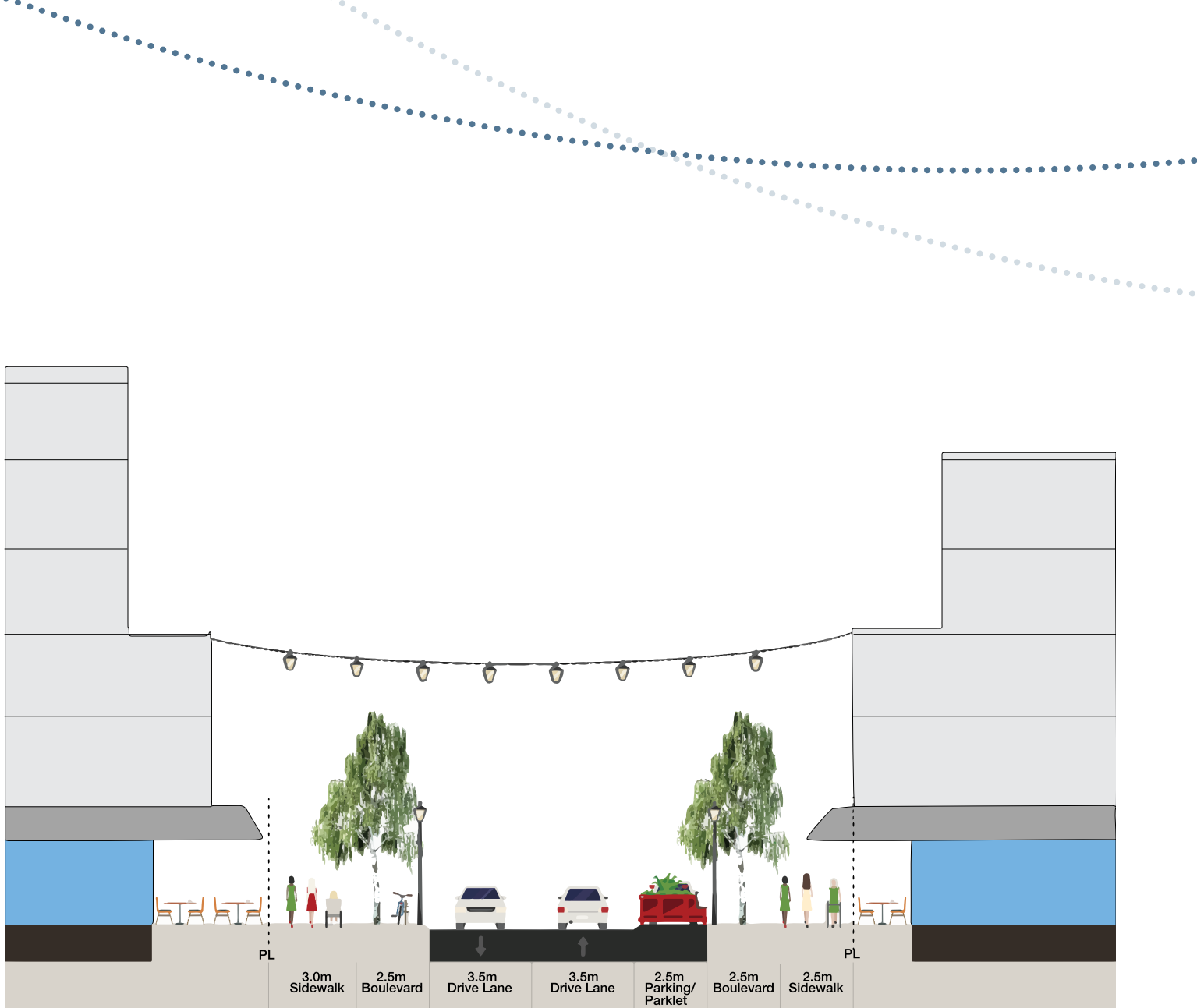


Figure 7.9: Conceptual cross section for Oak Street (20 metres)



Oak Street

7.9 AUDLEY CROSSING

Audley Crossing is a new laneway planned to be introduced through the development process. The lane will run between, and parallel to, Oak and Douglas Streets, commencing as an extension of Short Street and connecting all the way to Tolmie Avenue. Audley Crossing's primary intentions are to break up the large blocks and massing of surrounding developments, enable adequate on-site servicing, loading and access and provide additional opportunities for ground-oriented residential and commercial uses. The desired placement of the lane will align with and link the future community park acquisition and Audley Transit Station and provide opportunities for added connections to activated public space.

Audley Crossing will be designed as a pedestrian-oriented, activated shared street, bringing character while ensuring functionality of the space through the use of reduced speeds, distinctive and varied paving materials, landscaping, lighting and other features. The lane will contribute to making this an attractive and inviting public realm while retaining the shared purpose of service and access.

POLICIES

- 7.9.1 Create a new laneway, Audley Crossing, through the development process, generally based on the conceptual alignment identified on Figure 7.11.
- 7.9.2 Implement the conceptual design for Audley Crossing identified in Figure 7.10.
- 7.9.3 Design Audley Crossing as a shared lane with pedestrian priority, including features such as planters and living walls, street furniture and pedestrian lighting, on the buildings.
- 7.9.4 Define a two-storey street wall along Audley Crossing to frame the lane and reflect its pedestrian-scale character (See Figure 7.10).
- 7.9.5 Through the redevelopment process, work with developers to ensure optimal outcomes for Audley Crossing, including through easements, adjustments to the alignment and consideration of community contributions.
- 7.9.6 Encourage opportunities for integrating Audley Crossing as part of large redevelopment projects on Douglas Street and Oak Street, and support residential and commercial mixed-use developments (up to 3 storeys, stepped back at 2 storeys) that front onto the lane and serve to enhance its role, function and appearance as a shared thoroughfare.
- 7.9.7 Strongly encourage active commercial developments along Audley Crossing that frame the lane, provide commercial activity and pedestrian orientation and offer a spilling-out of uses into the space (e.g. cafes, patio seating).
- 7.9.8 Provide pedestrian, bicycle, local vehicle and loading/delivery access while creating an exceptional pedestrian-oriented lane that accommodates recreational and social activities.
- 7.9.9 Expect all loading access and bays to be on-site and integrated into building design (see guidelines 9.2.5 ii).
- 7.9.10 Consider public open space linkages (i.e. new park acquisition) and other improvements that create unique areas along Audley Crossing including pocket parks, enhanced seating areas and public art.

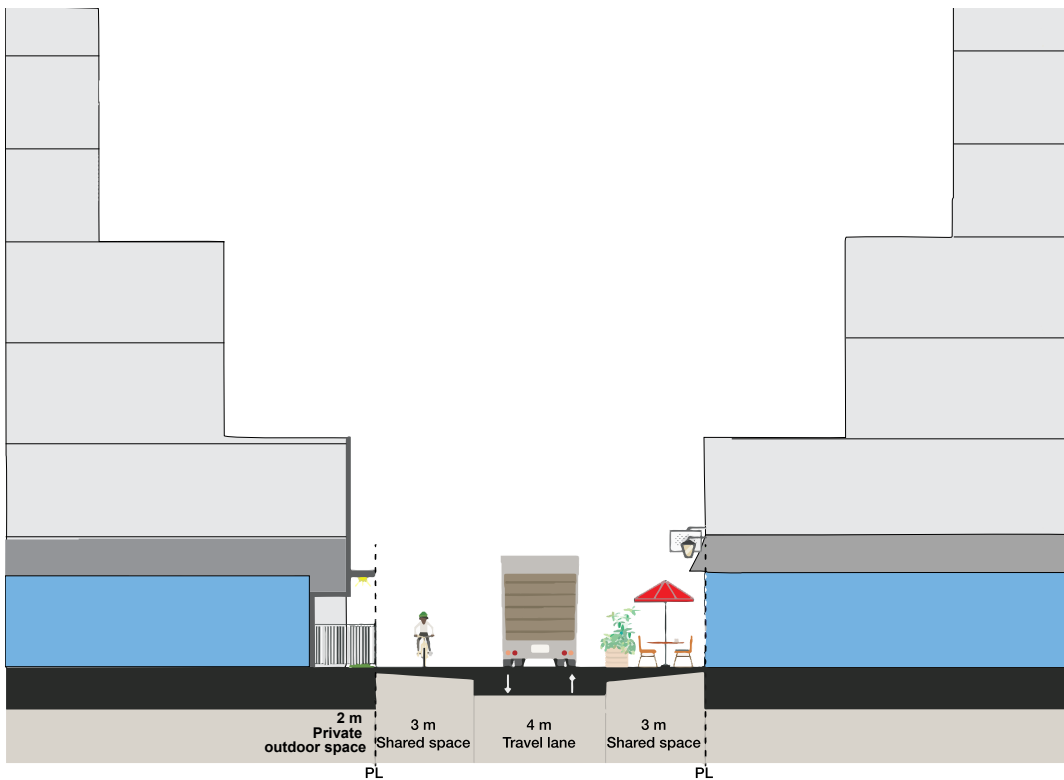


Figure 7.10: Conceptual cross section for Audley Crossing (10 metres)

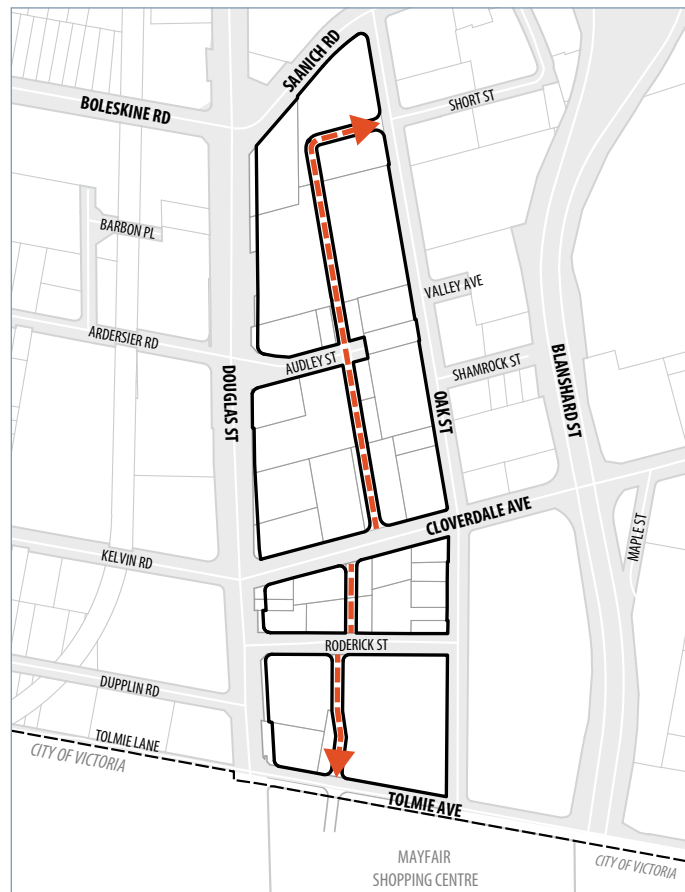


Figure 7.11 Conceptual alignment of Audley Crossing

8. PARKS, OPEN SPACES, TRAILS AND COMMUNITY FACILITIES

The importance of green space and access to recreation opportunities in urban contexts cannot be overstated. Access to public open space, parks, natural areas and playgrounds creates numerous health, environmental and social benefits.

Similar to many other municipalities in Canada, park space in Saanich has typically been focused on serving residential populations in suburban neighbourhoods. The Uptown–Douglas (UD) area represents a much different context than most other areas of Saanich. The vast majority of future residential development will be in the form of multi-unit housing, with limited private outdoor space. Additionally, the area has three times as many employees as residents, creating a much different park user profile than many other areas of the District. In order to continue to attract investment, support commercial activity and improve the quality of life for everyone in the area, parks and open spaces that service a variety of populations will need to be provided.

Generally speaking, the UD area is deficient in parks when compared with other neighbourhoods and Saanich parks standards, with the least park space per person of all areas

of Saanich. However, there are several public gathering spots in the UD: the Greater Victoria Public Library (Uptown Shopping Centre); Rutledge Park; Rudd Park; Uptown Plaza; and Mount View Park. As the population in the area grows so too will the need for open space and community facilities.

This section details how future population and employment growth will be supported through the introduction of new parks, trails and open spaces that will provide recreational opportunities, animate public space and better connect key destinations. Emphasis will be placed on creating new parks and open spaces in the centre of the plan area where significant growth is anticipated and there is an existing shortage of park space. These large park acquisitions will be achieved through the Development Cost Charge (DCC) Program, while smaller scale, local serving parks and public open spaces will be acquired through opportunities presented at the time of redevelopment. Collectively, these acquisitions will form a robust and varied network of spaces that is critical to a thriving, complete community.

Objectives

- A. Create a series of parks, plazas, open spaces and streetscape redesigns highlighted by a central civic space.
- B. Provide park space, including play areas, meeting places, tree cover, natural area parks, greenways and trail systems to serve the existing and future population.
- C. Retain and enhance areas of ecological value and connect natural areas.
- D. Promote the important role that parks, recreation and culture contribute to the quality of life and economic development of communities.
- E. Supply additional parks, open spaces and community facilities to meet existing needs and to support the future needs of the community.
- F. Consider a diversity of user needs and abilities for the park and open space network.



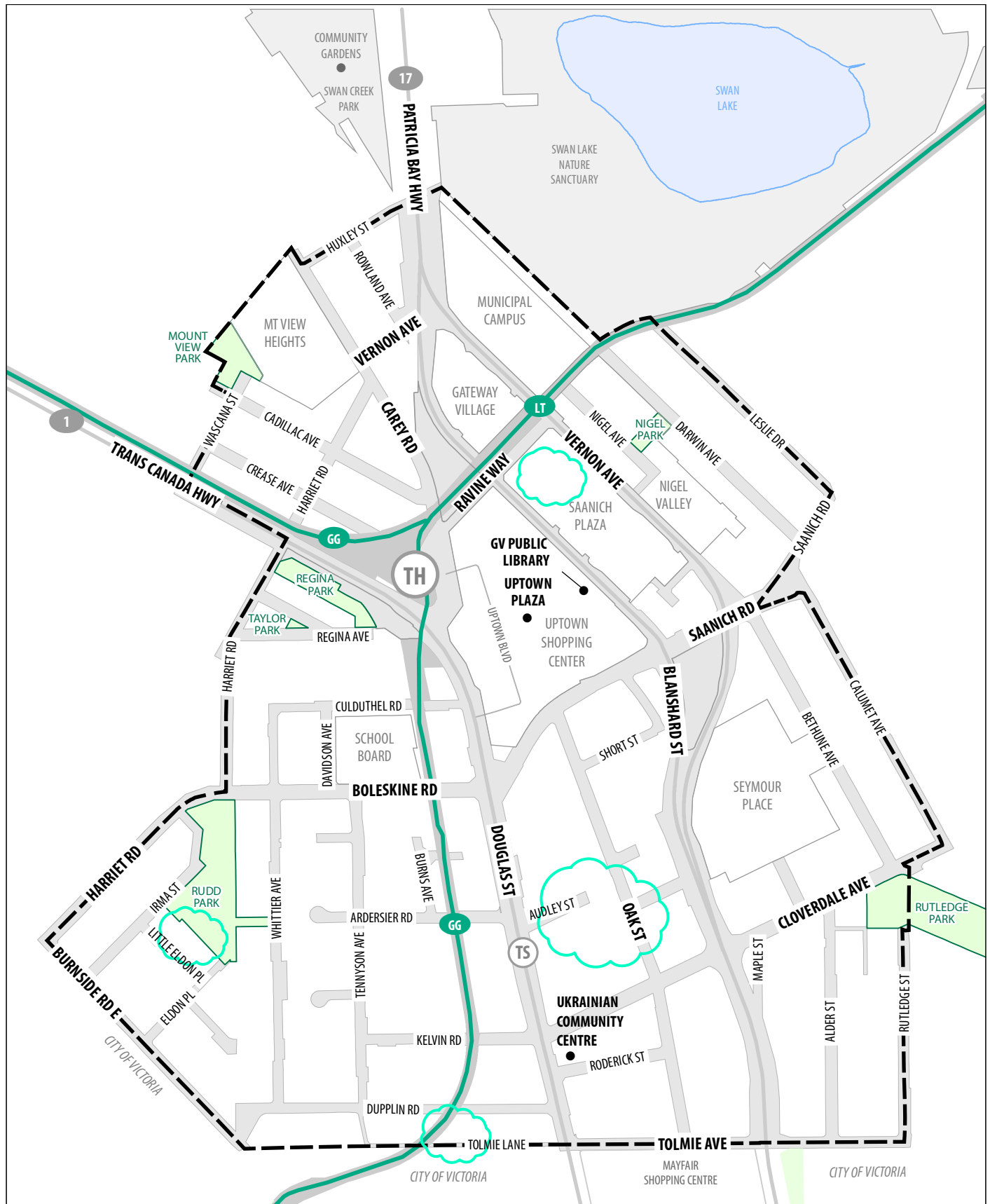
8.1 PARK ACQUISITION AND FINANCING

Based on the current park supply and projected population and employment growth, the Uptown–Douglas (UD) area has the greatest need for additional park space in all of Saanich. Significant new park space is foundational to support quality of life for new and existing residents. In order to fulfill anticipated need in this diverse urban area, a hierarchy of parks and open spaces with a range of sizes and functions will be required (See Figure 8.1).

A central civic space in close proximity to the Municipal Campus and future transit hub and a sizable community park close to Oak Street / Audley Crossing are significant building blocks to the envisioned network of parks and open spaces in the UD area. Smaller scale neighbourhood parks and publicly accessible open spaces and plazas will complete the network, supporting higher density development and contributing to a vibrant urban environment.

POLICIES

- | | |
|--------|--|
| 8.1.1 | Work towards a network of parks and open spaces within the UD (Figure 8.1) that provides: <ul style="list-style-type: none">• A community park within 1000 metres (10-minute walk) of all properties;• A neighbourhood park or plaza / urban park within 500 metres (5-minute walk) of all properties; and• A publicly accessible open space within 300 metres (2-3 minute walk) of all properties. |
| 8.1.2 | Use the Development Cost Charge program to finance the acquisition of new community park space to support population and employment growth. |
| 8.1.3 | Prioritize new community park space acquisitions, as noted in Map 8.1, as follows: <ul style="list-style-type: none">• A central civic space, at least 0.5 hectares in size, in close proximity to the Municipal Campus and future transit hub;• A community park to support residential growth along Oak Street / Audley Crossing and a new major transit station at Audley Street;• Additional park space to enhance the role and function of Rudd Park and enable it to support future development; and• Park space that animates the Galloping Goose Regional Regional Trail and provides significant environmental benefits. |
| 8.1.4 | Use a minimum standard of 1.2 hectares of neighbourhood parks per 1,000 people to guide acquisition. |
| 8.1.5 | As part of any redevelopment application, seek to acquire land for neighbourhood parks, plazas/urban parks (non-DCC parks), and/or publicly accessible open space based on the gross land area of the total lot, as follows: <ul style="list-style-type: none">• Minimum 10% for properties 1 hectare or more, with a minimum 50% of this area to be formally dedicated as Park;• Minimum 5% for properties between 0.5 and 1 hectare, with no formal dedication of Park required. |
| 8.1.6 | Support the addition of pocket parks, plazas, and the general enhancement of streetscapes in all proposed development. |
| 8.1.7 | Consider private / public partnerships to acquire, develop and maintain parks, trails and community facilities. |
| 8.1.8 | Consider the use of covenants, development agreements, community amenity contributions, housing agreements, density bonusing, and variances as possible mechanisms to secure public amenities and open spaces including plazas and playgrounds during the application review process. |
| 8.1.9 | Collaborate with the Capital Regional District, Ministry of Transportation and Infrastructure and City of Victoria regarding potential for added parkland adjacent to the Galloping Goose Regional Trail at Tolmie Lane. |
| 8.1.10 | Consider using undeveloped road rights-of-way to further enhance the park network and overall network connectivity. |



-  UPTOWN DOUGLAS PLAN
-  UDC PARK
-  PRIORITY AREA FOR PARKS
-  COMMUNITY FACILITIES / POINTS OF INTEREST

Parks & Open Space Type	Purpose	Size	Walking Distance Guideline	Examples
Private Open Space	To provide outdoor space for private property/strata developments.	Various sizes	On-site	Balconies Patios Courtyards Backyards Gardens Green Roofs Rooftops Common outdoor spaces
Publicly Accessible Open Space (private ownership)	To increase the availability of publicly accessible open space, including places for residents, employees and visitors to gather on private, publicly accessible open space developments.	Various sizes	2-5 minutes	Uptown Shopping Centre Plaza, Community Gardens
Plaza / Urban Park	To increase public gathering places throughout the area in a variety of urban forms, complete with public amenities and decorative elements.	Various sizes	5 minutes	Plaza, Square, Urban Park
Neighbourhood Parks	To provide local recreation opportunities, including play equipment, pathways, open grass, seating around play environments or areas of refuge for residents and employees within 400 metres.	Various sizes	5 minutes	Mount View Park, Regina Park
Community Parks	To provide spaces with high quality elements such as sports fields, playgrounds, skateparks, trails, urban plazas and/or natural features for users 1 to 3 kilometres away.	0.5 ha and larger	10-20 minutes	Rudd Park, Rutledge Park
Municipal Parks	To provide a range of park uses, including natural features such as beaches and forests for all Saanich residents.	20-200 ha	n/a	Swan Lake Nature Sanctuary

Figure 8.1: UD Parks and Open Space Framework

8.2 PARKS, OPEN SPACES AND TRAIL NETWORK DESIGN

The design of cities and neighbourhoods affects the ease with which people can walk, cycle, participate in active recreation and interact with neighbours and their community. With an increase in multi-unit housing in the Uptown–Douglas (UD) area, ensuring access to well-planned parks and trails will foster a sense of community, promote healthy

lifestyles and connect destinations. The type of open space will range greatly within the UD area from multi-use trails and greenways to urban plazas, community gardens and natural spaces, each connected to create a complete network for the community.

POLICIES

- 8.2.1 Support the development of additional public parks, open spaces and trails to provide amenity space throughout the area and strengthen the green corridor network identified on Map 4.2.
- 8.2.2 When considering new parks or publicly accessible open spaces as part of development proposals or capital projects, seek to locate and design spaces to:
 - Optimize connectivity to active transportation networks;
 - Complement or expand existing parks or public spaces;
 - Address gaps in the parks and open space network, enhancing walkable access;
 - Add new connections to improve walking and cycling;
 - Maximize sunlight and minimize potential impacts from shading; and
 - Be supported by active uses, such as coffee shops, restaurants or community facilities.
- 8.2.3 Enhance the design of municipal streets and rights-of-way to better link parks, open spaces and plazas.
- 8.2.4 Support the Capital Regional District in moving towards separate cycle paths for cyclists along the Galloping Goose and Lochside Regional Trails.
- 8.2.5 Ensure parks, plazas and other publicly accessible open spaces are well-maintained and safe and provide varied amenities to participate in social activity (consider children, youth, and seniors).
- 8.2.6 Increase the profile of the Municipal Campus site and adjacent Gateway Village/Saanich Plaza sites, including through the development of a central civic space that is visually prominent, multi-purpose and easily accessed by all modes of travel.
- 8.2.7 Support options for the future of Regina Park, including a re-design or land swap, that would improve the functionality of the park and reduce the impact of the Trans-Canada Highway (see Policies 4.5.3 and 5.13.4).
- 8.2.8 Pursue enhancements to Regina Park that enrich the recreational and ecological value of the park, including enhancement of the urban forest and introduction of community facilities (see Policies 4.4.4 and 5.13.4).
- 8.2.9 Work with the Provincial Government to explore options to achieve greater public benefit from the MOTI highway islands (on Blanshard Street north of the Lochside Regional Trail and south of Saanich Road).
- 8.2.10 Support opportunities to incorporate food producing trees and plants in public areas where appropriate (see also Section 4.1 and Policy 10.5.1).
- 8.2.11 Consider community gardens within existing or proposed parks, institutional lands, undeveloped parcels, and closed road rights-of-way, as per the Community Gardens Policy, where appropriate.



8.3 GALLOPING GOOSE AND LOCHSIDE REGIONAL TRAILS

The Galloping Goose Regional Trail was originally a freight railway line built during the First World War. Dedicated to recreational use in 1989, it is part of The Great Trail, a national multi-use trail system linking trails from coast to coast. “The Goose” intersects with the Lochside Regional Trail, a 29-kilometre former railway line from Saanich to Sidney. It also intersects with the E&N Rail Trail – Humpback Connector, a new 17-kilometre cycling and pedestrian trail being constructed largely within the E&N rail corridor that links Victoria and the Westshore communities.

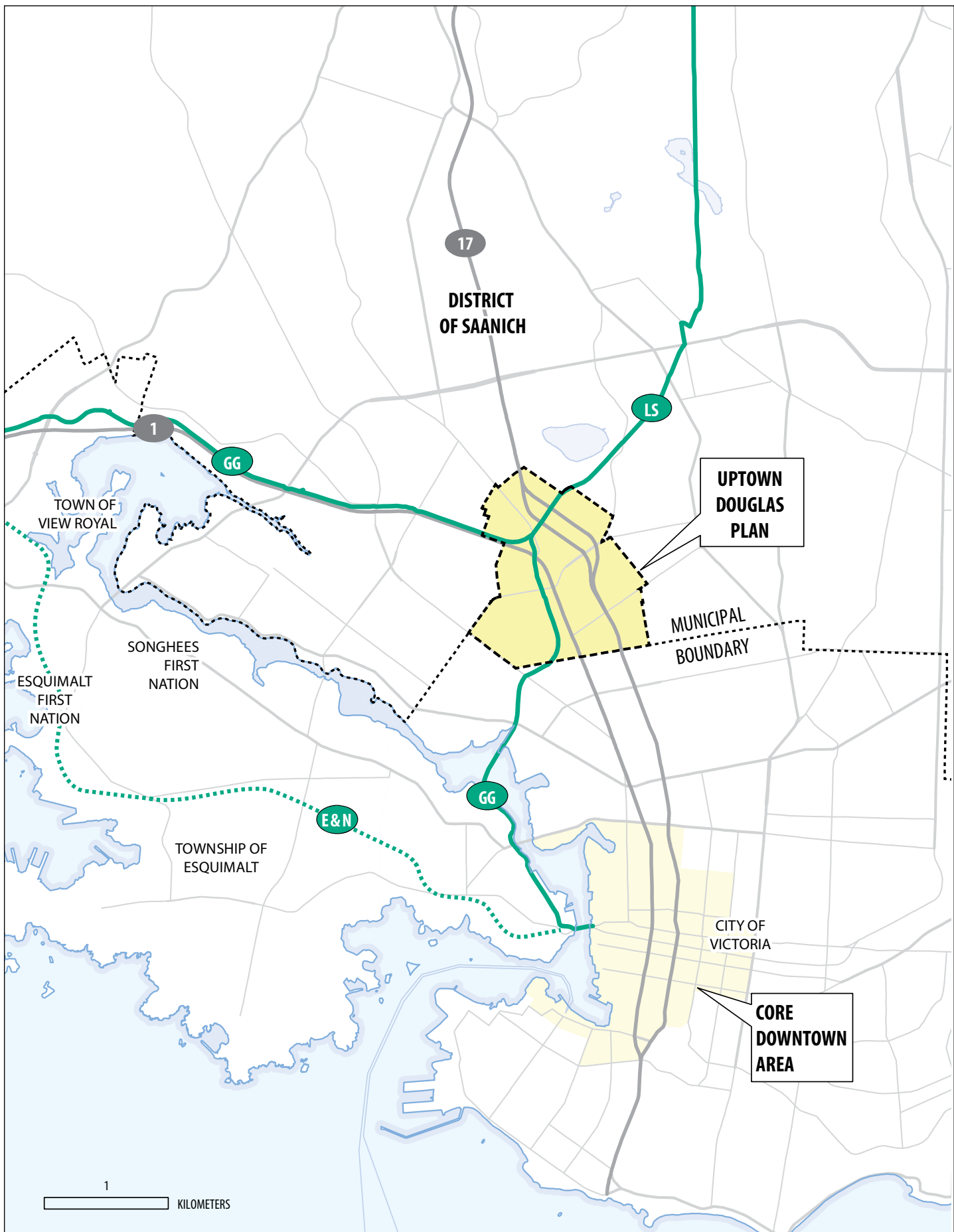
The corridors that contain the Galloping Goose and Lochside Regional Trails within the Plan area are owned by the Province of BC, administered by the Ministry of Transportation and Infrastructure (MOTI) and leased by the Capital Regional District's (CRD) Regional Parks Division,

where development, operation and maintenance is overseen.

The Galloping Goose and Lochside Regional Trails are integral components of the active transportation network, providing transportation and recreation opportunities for residents and visitors alike. As usage of the trails increases over time, Saanich and the CRD will continue to work together to address changes and improvements to ensure the safety and comfort of all users. Additionally, the trail represents a key asset in the Uptown-Douglas (UD) area – one that can be further animated through adjacent land use changes and better integrated through new points of access to advance social well-being, economic development and environmental health.

POLICIES

- | | |
|-------|---|
| 8.3.1 | Work with the Capital Regional District to enhance the Galloping Goose and Lochside Regional Trails by expanding the width of the trail and creating separate areas for pedestrians and cyclists to improve safety and enhance the experience for all users. |
| 8.3.2 | Work with the CRD to explore opportunities to improve the safety and visibility of access points and crossing locations along the Galloping Goose and Lochside Regional Trails, through: <ul style="list-style-type: none">• Focusing on high use/high conflict areas;• Addressing sightlines and signage needs; and• Improving road-trail crossings, including through marking and lighting. |
| 8.3.3 | Pursue the development of pocket parks or plazas along the Galloping Goose Regional Trail (see Policy 5.8.6). |
| 8.3.4 | Work with the Capital Regional District to enhance wayfinding to better link regional trails with key destinations and active transportation routes. |
| 8.3.5 | Work with Capital Regional District and adjacent property owners to enhance public amenities along the Galloping Goose and Lochside Regional Trails, including through the provision of shelters, water fountains, benches, washrooms and bike kitchens. |
| 8.3.6 | Support public art installations on private property along the Galloping Goose and Lochside Regional Trails, to celebrate and animate the area. |
| 8.3.7 | Enhance connectivity to the Lochside Regional Trail and support the creation of a public/ open space at the Municipal Campus site that complements the Nigel Valley Plaza as part of upgrades or redevelopment (see Policies 8.2.6 and 5.12.11). |
| 8.3.8 | Work with the Capital Regional District and MOTI to enhance the integration of the Galloping Goose and Lochside Regional Trails with adjacent land uses, including exploring additional non-motorized access points to uses fronting on to the Trail. |
| 8.3.9 | Ensure redevelopment proposals adjacent to the Galloping Goose and Lochside Regional Trails are designed to: <ul style="list-style-type: none">• Improve passive visual surveillance while maintaining the greenway character of the Trail;• Add to interest and enhance the recreational experience of the trail;• Address potential conflicts created by access points; and• Include landscaped areas adjacent to the Trail with a balanced focus on native and drought tolerant plants to enhance the greenway character of the trail corridor. |



Map 8.2 Regional Trail Network

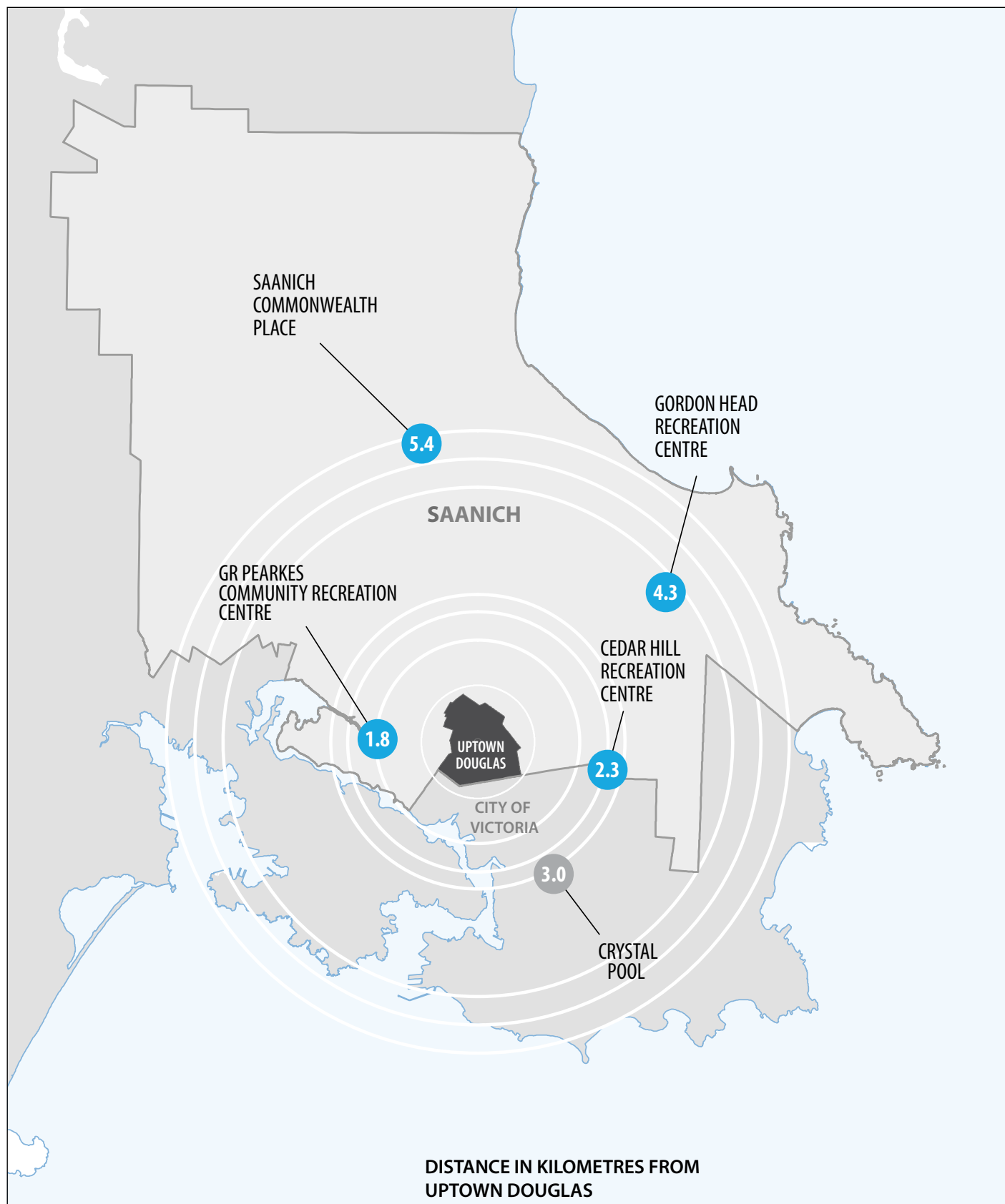
8.4 COMMUNITY FACILITIES

The Uptown–Douglas (UD) area does not contain any major Saanich Recreation Centres. However, Pearkes Recreation Centre, Commonwealth Recreation Centre and Cedar Hill Recreation Centre can all be accessed within a 10-minute drive (See Map 8.3). While the facilities currently in Saanich meet the needs of today's population, additional growth in the UD area may provide a long term rationale to explore the addition of a new recreation facility in the area. Easy access to transit and the Galloping Goose and Lochside Regional Trails combined with a high density of residents and employees would make this area an ideal fit for a community recreation centre when the need is warranted.

While pursuing a larger community facility in the long term is desirable, additional community facilities will also need to be added incrementally to help support population growth and provide the recreational opportunities that are critical to the quality of life of residents, employees and visitors. These incremental changes can include adding smaller scale or privately owned community facilities and improving the connectivity to and accessibility of existing facilities. Needs expressed by community members include a new school, meeting spaces and daycare.

POLICIES

- 8.4.1 Review recreational programming and facilities in the context of projected population growth for the UD planning area and directions from the Recreation Market Analysis to ensure they are meeting emerging needs.
- 8.4.2 Support the long term introduction of a new community recreation facility within the UD area in a location that has convenient access to transit and active transportation networks.
- 8.4.3 Explore possible public-private partnerships as a means to add new recreation facilities.
- 8.4.4 Liaise with the Library Board regarding implementation of its long-range facility plan, and support the concept of co-location of libraries and community centres.
- 8.4.5 Support the enhancement of library facilities in the area, including through development of a new facility that has greater floor space, is more prominently located in the area and is easily accessible by foot.
- 8.4.6 Work with the School Board to accommodate long-term needs for school facilities in the area to respond to anticipated growth in population and school age children.
- 8.4.7 Support the exploration of a new public school within the UD, with a priority on the School Board site at Boleskine Road as a potential future location (See also Policy 5.13.2).
- 8.4.8 As part of redevelopment applications, pursue community facility amenity contributions, including daycares, community centres or community meeting spaces.
- 8.4.9 Explore the introduction of youth-oriented facilities and amenities in Rudd Park.
- 8.4.10 Support the establishment of a youth centre in the area, potentially as a component of a multi-unit residential, mixed-use or commercial development.
- 8.4.11 Explore the potential introduction of a community recreation facility and/or library as a potential component of any redevelopment of the Municipal Campus site.



Map 8.3 Recreational Facilities

9. URBAN DESIGN



Uptown Shopping Centre

The Uptown–Douglas (UD) area has a concentration of economic activity and a diversity of land uses that are conducive to an animated and vibrant urban environment. However, the auto-oriented design of major roads and buildings creates barriers to a quality public realm. Individual assets such as Rutledge Park and the Uptown Shopping Centre have some design elements that foster social interaction, but the majority of land uses are more utilitarian in form and function and offer limited contributions to the overall character of the built environment. Additionally, the overall cohesiveness of the area is limited, with a poor quality public realm, a lack of pedestrian-oriented building design and minimal elements that reinforce the area's position as a regional centre.

Recognizing the current economic value of the area, policies and principles seek to further advance the UD area as a leading regional destination while enhancing its character. A focus on pedestrian-oriented development and multi-modal design will be critical to this transition. Mobility for all ages and abilities is a clear priority in the Plan area with policy that further defines and supports future rapid transit and bicycle, pedestrian and vehicle mobility.

This section is divided into two parts. The first part (9.1) identifies the guiding principles of urban design that has informed the later section. The second part (9.2) provides design details for specific elements of site layout, built form and public realm.

OBJECTIVES

- A. Support the creation of thriving places, animated spaces and enhanced streetscapes through the application of the pedestrian-oriented, transit-oriented and multi-modal design principles.
- B. Encourage placemaking and context sensitivity through the delivery of thoughtfully designed buildings, streets, interfaces and open spaces.
- C. Promote high quality, diverse building designs that are a showcase of quality architecture and feature innovative, durable materials.
- D. Transition form and massing of buildings ensuring heights and overall densities transition down and outwards from the core to lower density neighbourhoods.
- E. Encourage site planning and sustainability features in new developments that contribute to the District's sustainability targets.
- F. Expand green spaces and environmental stewardship through application of sensitive and holistic site design.
- G. Frame and retain public views through thoughtful consideration of building placement, adjacencies, special corridors and vistas.

9.1 GENERAL URBAN DESIGN POLICY

The process of designing and shaping our cities requires collaboration of many professions that approach the built environment through varying lenses. The goal of good urban design is to direct the appearance and built form of our neighbourhoods, streets and spaces and extend its influence to the cultural, economic, environmental and social values of design. With considerable opportunity for redevelopment

in the Uptown–Douglas (UD) area, the intention is to have new developments lead growth with positive changes in the community, including quality architecture, comfortable streetscapes and welcoming public spaces that are accessible and well connected. The urban design objectives will be realized, broadly, through the following policies.

POLICIES

- 9.1.1 Integrate the UD Principles and Framework (Section 9.2) into the development of updated Development Permit Area Design Guidelines.
- 9.1.2 Apply the Urban Design Principles identified in Section 9.2 as the guiding framework to assess the form and character of redevelopment applications.
- 9.1.3 Apply the principles of Crime Prevention through Environment Design (CPTED) in site planning, organization and building design.
- 9.1.4 Apply the standards of universal design and accessibility into all design elements of development.
- 9.1.5 Support more sustainable management of urban stormwater, using alternatives to piped systems such as rain gardens and bio-swales.
- 9.1.6 Support public art, as an independent installation in a public space and/or as a component of the building design.
- 9.1.7 Transition building height down from the highest density within the Douglas-Oak Hub out towards the lower density edges of the Plan boundary, as shown in Figure 9.1.
- 9.1.8 Incorporate public view corridors into new development including through building siting, separation and transition in massing and height. Important view corridors are identified on Map 9.1 and include:
 - i. Olympic Mountains;
 - ii. Strait of Juan de Fuca;
 - iii. Victoria skyline;
 - iv. Sooke Hills; and
 - v. Christmas Hill.
- 9.1.9 Gateways into the UD should be defined and celebrated through special signage, landscape elements, quality surface treatment, unique lighting and public art. Strategic locations are referenced on Map 9.1 and include:
 - i. Douglas Street at Carey Road;
 - ii. Vernon Avenue and Blanshard Street at Patricia Bay Highway; and
 - iii. Tolmie Avenue at Douglas Street and Blanshard Street.
- 9.1.10 Develop a wayfinding strategy for Uptown-Douglas, as part of Saanich's overall wayfinding program, with a focus on connecting major destinations and integrating active transportation and transit networks, including the Galloping Goose Regional and Lochside Regional Trails.



Figure 9.1: Transition building heights down from the Core



South/Southwest views: Olympic Mountains, Juan de Fuca Strait and Downtown Victoria



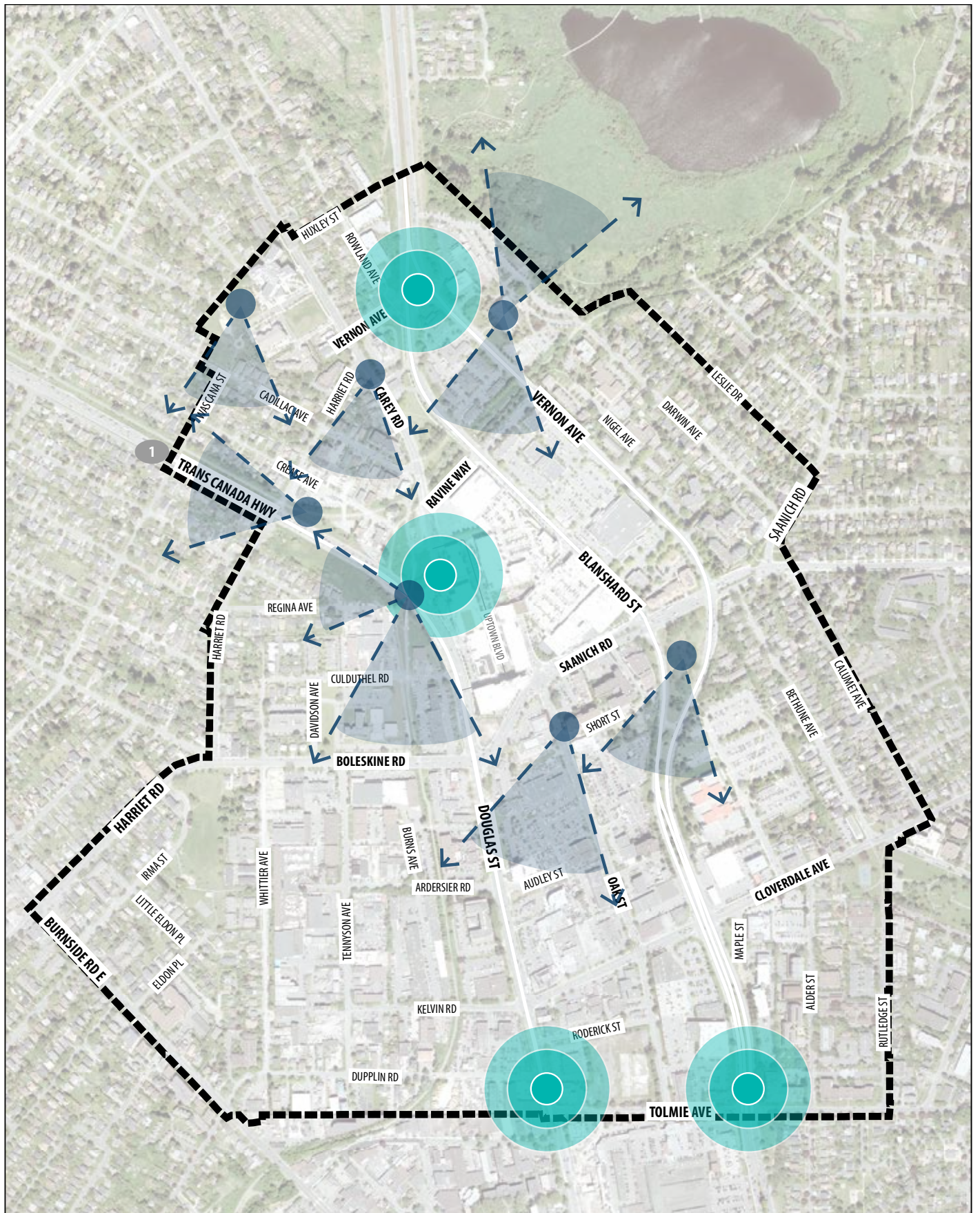
West views: Towards Sooke Hills



South views: Victoria and Olympic Mountains



North/Northeast views: Christmas Hill, Mount Douglas and Swan Lake Estuary



Map 9.1 Public View Corridors and Gateways



9.2 URBAN DESIGN FRAMEWORK

The purpose of the design framework is to guide the transition of space to place and develop a place that is unique, interesting and in keeping with the Plan's vision. It promotes community, liveability, aesthetics, ambiance, culture, safety, accessibility and an environment that is inclusive to all age groups and abilities.

Furthermore, the Urban Design Framework seeks to develop a high density urban core while highlighting and enhancing the unique characteristics and functions of each neighbourhood sub-area (See Sections 3.4 and 5 for sub-area objectives and land use policies). New developments, street improvements and the introduction of public spaces will fuel the ability to generate a desired sense of place within each sub-area and within the larger urban core as a whole.

Public realm is a focus of this Plan, with land use and design objectives supporting the development of vibrant areas for social interactions on a myriad of scale, including streets, plazas, public squares, parkettes, and pocket parks. Improvements to the parks and open space network, both new and enhanced, have been identified as a critical piece of infrastructure needed to meet the anticipated needs of this fast developing urban area. Improving streetscapes through safety and beautification is no less important to improving the quality of the pedestrian experience in the UD area.

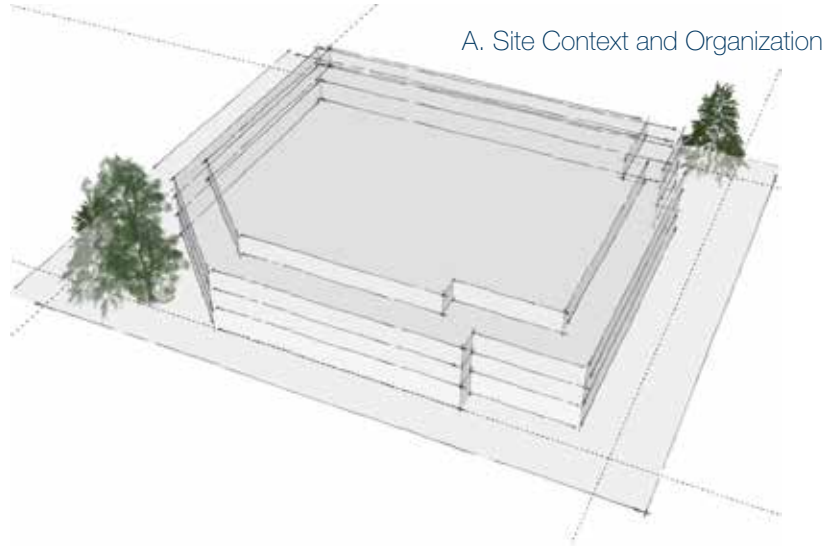
Urban design directions are intended to be used in conjunction with directions in Section 5 (Land Use) and Section 7 (Significant Streets). The content in these three sections, in particular, collectively work together to articulate the Plan's vision for the design of the built environment.

The framework is divided into three sub-sections, namely:

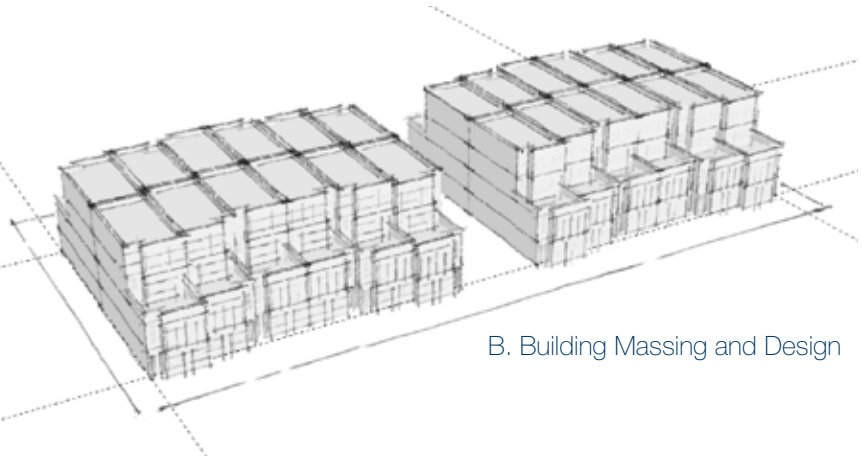
- A. Site context and organization;
- B. Building massing and design; and
- C. Public realm.

Drawings provided within this section seek to illustrate the guidelines and act as examples only.

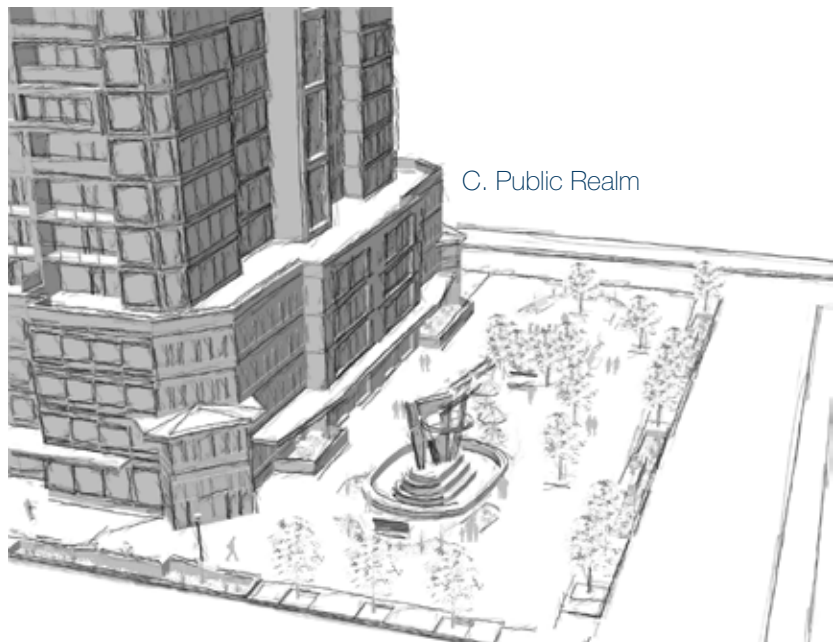
The intent of the drawings is to assist designers to further understand the concept and work within the framework to arrive at a creative design solution. Since the relative importance of the specific guidelines will vary for each project, and as there are many ways of meeting each guideline, individual applications will be evaluated on their own merits. Creative solutions that meet the intent of the Guidelines are encouraged.



A. Site Context and Organization



B. Building Massing and Design



C. Public Realm

URBAN DESIGN FRAMEWORK ELEMENTS

A. SITE CONTEXT AND ORGANIZATION

9.2.1 Context Analysis: Evaluate the existing and planned site context and demonstrate how the proposed design and layout optimizes outcomes respecting building energy performance, improved livability and localized assets.

- i. Site design should incorporate tree retention and protection and identify strategies for tree replacement and integration of landscape features that increase the green areas on site.
- ii. Passive design principles should be incorporated into site planning and design, including through consideration of building siting, solar orientation, thermal bridge-free design, shading/ventilation measures and other sustainable design features (Figure 9.2).
- iii. During redevelopment, heritage buildings, sites and features should be a focal element of design considerations, including the preservation and integration of such as part of site planning and organization.

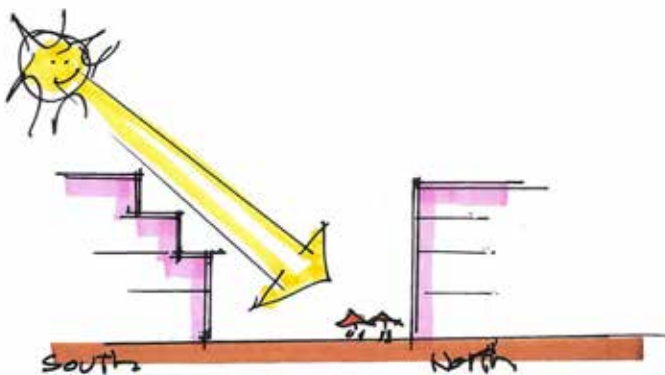


Figure 9.2: Optimize solar orientation of buildings and public spaces

- iv. New construction should be compatible with adjacent heritage buildings and complement existing heritage materials, pattern and scale of the streetscape by providing an appropriate transition between differing scales and heights of neighbouring buildings.
- v. Large sites with multiple buildings and the potential for new public realm elements should consider coordinating development through a Master Plan. Potential sites include the School

Board Site, Saanich Plaza, Gateway Plaza, 4000 Seymour and the Municipal Campus.

- vi. Site design should promote and facilitate activity and social interaction through planned features such as active uses, strategic connections and public open spaces.

9.2.2 Building Placement: Locate buildings to frame the edges of streets, parks, and open space. Ensure that buildings fit harmoniously with the existing context and provide opportunities for high-quality landscape and streetscape design.

- i. Building types and forms should respect the scale and character of existing and planned land use and respond appropriately to the site conditions, context and adjacencies.
- ii. Building orientation should consider generalized and proximate view corridors and sightlines, including those identified on Map 9.1.
- iii. Buildings located at the end of terminating views and street corners should generally emphasize vertical articulation and include prominent architectural features (See Figure 9.3a).
- iv. Street edges should consider setbacks for urban squares and plazas to enable animation and reduce negative impacts of shadowing on parks and other public spaces.
- v. Buildings should be designed to contribute to an interesting, shaped, and distinctive skyline through articulated rooflines, upper storey massing and slender designs (See Figure 9.3b).



Figure 9.3a: Locate taller buildings at street corners

A. SITE CONTEXT AND ORGANIZATION

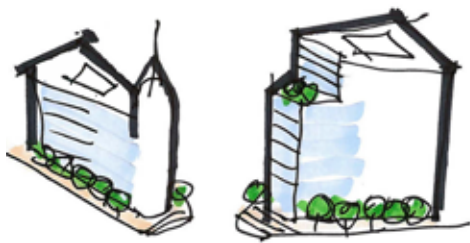


Figure 9.3b: Emphasize vertical articulation and include prominent architectural features

- vi. The intent of a front yard setback is to provide additional opportunities for building articulation, to step inward as building heights increase, and to establish a consistent street wall and building shoulder. Any projections into the front yard setback, beyond those noted within the Land Use section of this Plan (see Figure 5.1), must be in accordance with the District's Zoning Bylaw. In addition, development along a Significant Street (see Section 7) is expected to apply directions in the conceptual cross-sections. Typical setbacks should apply generally, as follows:

- **Neighbourhood Residential**

6 metres from property line to building face.

A **4 metre** minimum setback may be considered for developments fronting along Darwin Avenue and Calumet Avenue where each unit includes private outdoor space (e.g. patio, terrace) fronting the street that exceeds the minimum requirement (see Policy 5.2.7). The reduced setback must transition to the full 6 metre setback to provide relief as the building edges towards adjoining property lines (see Figure 9.4).

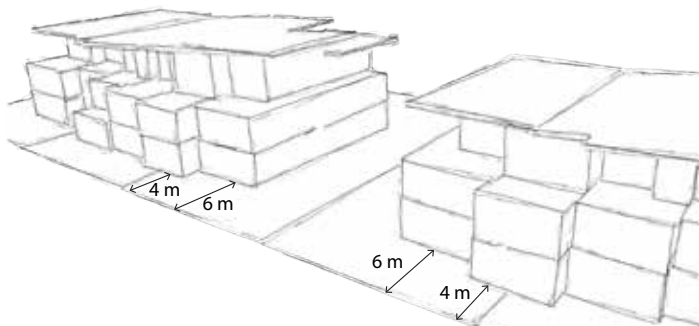


Figure 9.4 Neighbourhood residential setback

- **Neighbourhood Apartment Residential.**

4 metres from property line to building face.

A reduced setback may be considered for commercial uses at grade.

- **Mid-Rise Residential**

4 metres from property line to building face.

A reduced setback may be considered for commercial uses at grade.

- **Urban Mixed-Use Residential**

7 – 9 metres of clear pedestrian space from edge of curb to building face of the ground floor for developments fronting along Oak Street. The additional setback, beyond property line, should be inset to accommodate commercial-retail space and private outdoor space. Refer to Figure 7.9 (Oak Street cross-section).

A **5 metre** minimum setback from edge of curb to building face on the ground floor may be considered for developments fronting along Alder Street Carey Road, Cadillac Avenue, Cloverdale Avenue and Short Street where private and/or common amenity space exceeds the minimum (see Policy 5.2.5) and at grade residential units include private outdoor space fronting the street or at grade commercial units provide publicly accessible open space areas (square, café, etc.).

A **2 metre** setback from property line to building face on the ground floor will be considered for developments with frontage along Audley Crossing provided the space is either private outdoor space or public space as part of a commercial development (e.g. café seating). Refer to Figure 7.10 (Audley Crossing cross-section).

- **Core**

A **5 metre** minimum setback from property line to building face on the ground floor for developments fronting the Galloping Goose Regional Trail. The setback area is intended to be private or semi-public space and must include landscaping and other design elements that enhance the public realm.

- **Mixed-Employment**

A **5 metre** minimum setback from property line to building face on the ground floor for developments fronting the Galloping Goose Regional Trail. The setback area is intended to be private or semi-public space and must include landscaping and other design elements that enhance the public realm.

A. SITE CONTEXT AND ORGANIZATION

9.2.3 Public Realm Framework: Developments should provide a seamless transition into the public realm through enhanced integration with streets, parks, open spaces and adjacent development.

- i. Large blocks should be fragmented to accommodate new pedestrian and cycling connections, including but not limited to locations identified on Map 6.2.
- ii. Connections should be designed with appropriate landscaping and lighting and be a minimum of 5 metres.
- iii. Buildings adjacent to pedestrian connections should be setback to minimize shadowing on paths and create the perception of spacious public spaces.
- iv. Buildings should be designed to engage with sidewalks, accommodate commercial activity, invite pedestrian activity and offer passive open spaces (See Figure 9.5). In order to create street interfaces that are comfortable, buildings should generally be setback from the principal street as per Figure 5.1.



Figure 9.5: Design building to engage the street

- v. Transit shelters should be integrated into site and building design (See Figure 9.5). Where a shelter is independent from a building, a minimum of 2.5 metres for pedestrian movement from the rear of the shelter should be provided.

- vi. Buildings should be sited and designed to provide interesting views to surrounding features and spaces.
- vii. Amenity spaces, focal point and/or landmarks should be located strategically to create interest from public and private areas.
- viii. Courtyards, forecourts, plazas, parkettes, terraces and patios are encouraged to enliven the public and semi-public realm. Consider spill-over of these features to support vibrancy on streets and in public places (See Figure 9.6).



Figure 9.6: Incorporate building design features that enliven public spaces

- ix. Buildings should front public spaces and complement the surrounding context through use and desired experiences.
- x. Special paving, landscaping, seating/benches, bike racks and waste/recycling stations are encouraged to define the public realm, and to promote human-scale dimensions.
- xi. Public open spaces should be designed with soft transitions in grade and be barrier free.

A. SITE CONTEXT AND ORGANIZATION

9.2.4 View Corridors: Provide visual relief in the context of framing urban neighbourhoods, secure communal viewpoints through site planning and capture vistas through the windows of individual developments.

- i. Building form and massing (see Map 9.1) are expected to incorporate view corridors, illustrated through sympathetic orientation, design and massing.
- ii. Buildings should step down towards the edges of sites, blocks and neighbourhoods to maximize and preserve significant views.
- iii. Significant view corridors should be protected through strategic placement of higher buildings to frame views while other public views should be retained through open street ends/corridors, breezeways, internal roads and paths and other openings within the built form (See Map 9.1).



Figure 9.7: Design new buildings with integrated loading and servicing

9.2.5 Site Servicing, Access and Parking: Locate operational activities such as loading, servicing, utilities, storage and parking, underground, away from the public realm and screened from the public view.

- i. All parking must be located underground or underbuilding. Limited surface parking (e.g. accessible and visitor) may be considered at the rear of a building, and must be permeable surfaced and sufficiently screened from street view through landscape design features.
- ii. Building designs must include integrated loading stalls and bays, when loading is applicable to the use (See Figure 9.7).
- iii. Appropriately sized and conveniently located parking spaces should be provided in order to support a variety of transportation options including but not limited to carpools, car-shares, EV vehicles, and electric and cargo bicycles.
- iv. Residential development should include designated, secure and safe indoor bicycle and cargo- bicycle parking. Consideration for e-bicycle charging outlets are encouraged.
- v. All development should include designated, secure and safe outdoor bicycle parking complete with overhead shelter, placed near entries to buildings for enhanced connectivity.
- vi. Parking structures should consider safe pedestrian circulation networks through providing raised walkways, clear signage and appropriate lighting.
- vii. Developments should ensure access and egress to/from any adjacent parcels remains achievable for future development.

B. BUILDING MASSING AND DESIGN

9.2.6 Fit and Transition: Ensure buildings fit within the existing and planned context of the neighbourhood and provide appropriate massing, articulation and character, as well as transitions in form between various scaled buildings, parks and open spaces.

- i. Apply angular planes, minimum horizontal separation distances, and other building envelope controls to transition buildings down to lower-scale buildings, streets, parks and open spaces.
- ii. For Neighbourhood Residential, Neighbourhood Apartment and Mid-Rise Residential designations, angular planes, of approximately 45 degrees, should be applied to the building massing and design to enable sunlight to reach sidewalks, provide perception of more human scaled buildings, reduce overlook to neighbouring buildings and mitigate shadowing on public spaces (See Figure 9.8). At the back of a building, the angular plane should be measured from either the rear property line or the mid-line of a rear lane/street.
- iii. Transitions in massing should be provided through use of features including articulated facades, building setbacks, and shoulder setbacks on upper levels.
- iv. Building massing should step down when adjacent to a pedestrian connection.
- v. Corner locations should occupy the bulk of the building height and massing and be articulated to reinforce corridors and views to public places (e.g. streets, plazas).
- vi. Design should minimize the impact of shadowing and maximize access to sunlight, sky view, and privacy on neighbouring properties.
- vii. Building design should ensure that the effects of light pollution and placement of mechanical equipment (and its screening) does not negatively impact adjacent properties.
- viii. Design of new buildings should incorporate features that minimize negative impacts on bird populations, including but not limited to: glazing techniques, fritted glass, fenestration patterns, bird trap mitigation and reduced light pollution.

- ix. For buildings with a podium, storeys above should be massed and setback to allow light penetration and be articulated to create a pattern (e.g. saw-tooth) of slim, varying height forms.
- x. Buildings should be articulated to create a continuous streetwall, as detailed in Figures 7.1 – 7.10 and as shown on Map 9.2, generally as follows:
 - Mid-Rise Residential: 2-3 storeys
 - Urban Mixed-Use Residential: 2-4 storeys
 - Oak Street: 3 storeys
 - Audley Crossing: 2 storeys
 - Core: 4-6 storeys
 - Galloping Goose Regional Trail: 2 storeys
 - Mixed Employment: n/a
 - Galloping Goose Regional Trail: 2 storeys
- xi. Angled or terraced balconies, curtain or window wall systems, projecting windows, rooftop amenity space and other techniques are encouraged to promote public views and vistas and diversity in design.



Figure 9.8: Mid-Rise development showing 45° angular plane



Map 9.2 Street Wall Heights

B. BUILDING MASSING AND DESIGN

9.2.7 Primary Entrances: Ensure well-designed front entrances and front yards, offering privacy and notable form while maintaining connections and a perception of eyes on the street.

- i. Building entrances should be legible from the street, emphasized as a focal point of a building's facade and be placed in highly visible locations where there is the ability to animate a longer stretch of street (See Figure 9.9).
- ii. Entrances should be proportionally in scale with the buildings overall design and relate to the street.
- iii. Residential and office type uses with shared lobby entrances should be clearly differentiated and denote a sense of arrival through notable entry features (See Figure 9.10) including:
 - Landscape elements;
 - Prominent architectural features;
 - Transparent and double-height lobbies and entrances;
 - Canopies, awnings and other weather protection systems;
 - Integrated benches;
 - Visitor bicycle parking;
 - Appropriately scaled lighting; and
 - Clear signage.
- iv. Pedestrian scaled lighting should be integrated into building entrances, public spaces and in areas with high pedestrian traffic.
- v. Ground floor residential units adjacent to a street must provide at-grade individual entrances with direct connections to the public sidewalk. Consider the use of raised terraces, forecourts, patios, landscaping, screening, fences and gates to enhance individual residential entrances and create a semi-private transition to the street (See Figure 9.11).
- vi. Developments on corner lots are encouraged to locate building entrances at the corner to animate both streets.



Figure 9.9: Emphasize building entrances as a focal point



Figure 9.10: Create a sense of arrival at building entrances



Figure 9.11: Provide individual residential entrances with direct connections to the street

B. BUILDING MASSING AND DESIGN

9.2.8 Building Relationship to Streets: Developments should relate directly to the street level while blending with the topography of the natural and built form surroundings.

- i. Building faces should be designed for the human scale with main entrances that address the street and include open space to accommodate gathering.
- ii. Buildings should be located to define the street edge and create a continuous human-scaled street wall (See Figure 9.12).
- iii. Continuous weather protection is required along all commercial and mixed-use developments and is encouraged for mixed-employment developments.
- iv. On sloping sites, ground floor commercial areas should step with the grade to allow flush entry into units.
- v. Pedestrian connections should be flanked with active frontages.
- vi. For the Neighbourhood Residential designation, buildings should be articulated to provide 2 exterior walls for the majority of units; facades should be designed to be sympathetic to the surrounding context.
- vii. Development on corner sites (See Figure 9.13) should:
 - Align the building to the setback pattern of neighbouring buildings on both streets; and
 - Provide attractive finishing on both facades with emphasis given to the façade fronting the primary street.
- viii. Rear lot setbacks are intended to provide privacy and open space for residents and reduce overlook. These setbacks are dependent on building form, density and use and its adjacencies.
- ix. Buildings with commercial at grade should provide a range of unit sizes designed for active uses.

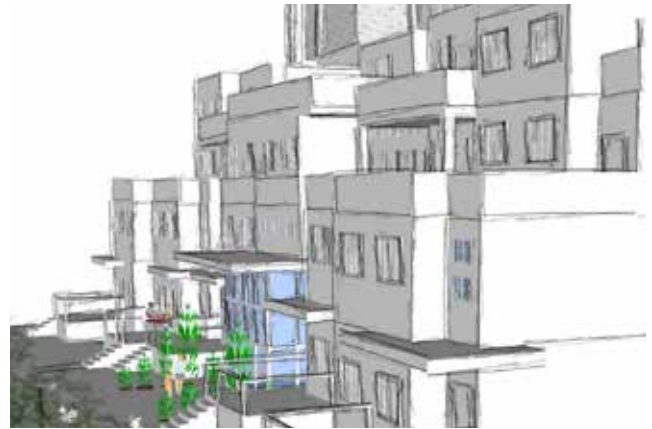


Figure 9.12: Use a continuous street wall to define the street edge

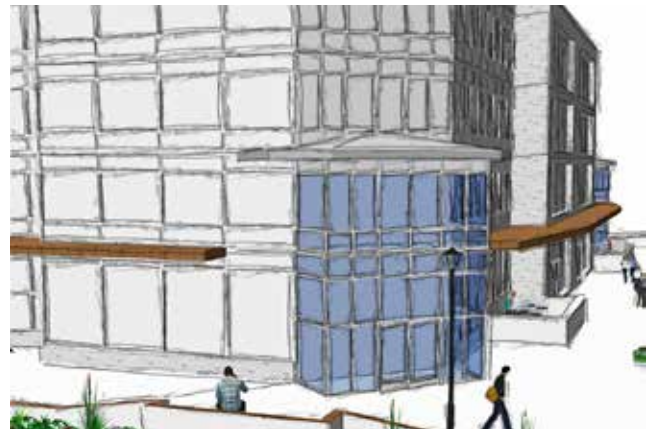


Figure 9.13: Address both frontages on corner sites

B. BUILDING MASSING AND DESIGN

- x. Small and transparent storefronts that increase the building definition and articulation are encouraged.
- xi. Within the Urban Mixed-Use designation, specifically along Oak Street, Individual commercial street frontages should not exceed 10-15 metres. However, large commercial units with small frontages may be considered on deep lots when the bulk of the floor area is located “behind” smaller, fronting units (See Figure 9.14).
- xii. Façades fronting streets and public spaces should have large, well-proportioned areas of glazing to enhance the streetscape and promote a sense of visual interaction between the building and public realm (See Figure 9.15).
- xiii. Ground floor commercial units should incorporate:
 - A minimum of 70% glazing; and
 - Primary doors, walls and windows that slide, stack, fold, collapse and retract are encouraged for active, at grade uses taking advantage of seasonal benefits and encouraging energetic streetscapes.
- xiv. Buildings should promote safe and convenient connections through development sites and mid-block pathways, especially in the Core and Urban Mixed-Use designations.
- xv. Signage should be designed to be consistent with the architectural style, scale and materials of the development and its surrounding context.
- xvi. No blank walls are permitted along street frontages (including the Galloping Goose and Lochside Regional Trails); temporary blank walls should incorporate articulations and/or public art (e.g. murals) for visual interest and engagement.
- xvii. Buildings and foundations (low or stepped foundation walls) should be designed to exclude or minimize the exposure of concrete foundation.

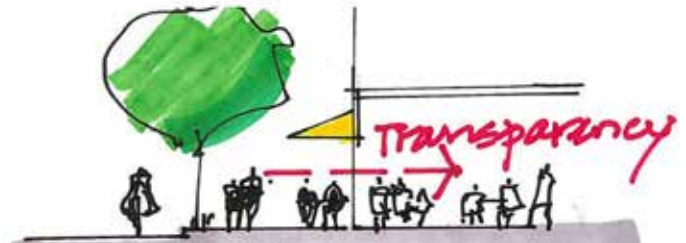


Figure 9.15: Activate the streetscape with appropriate glazing of at-grade commercial unit

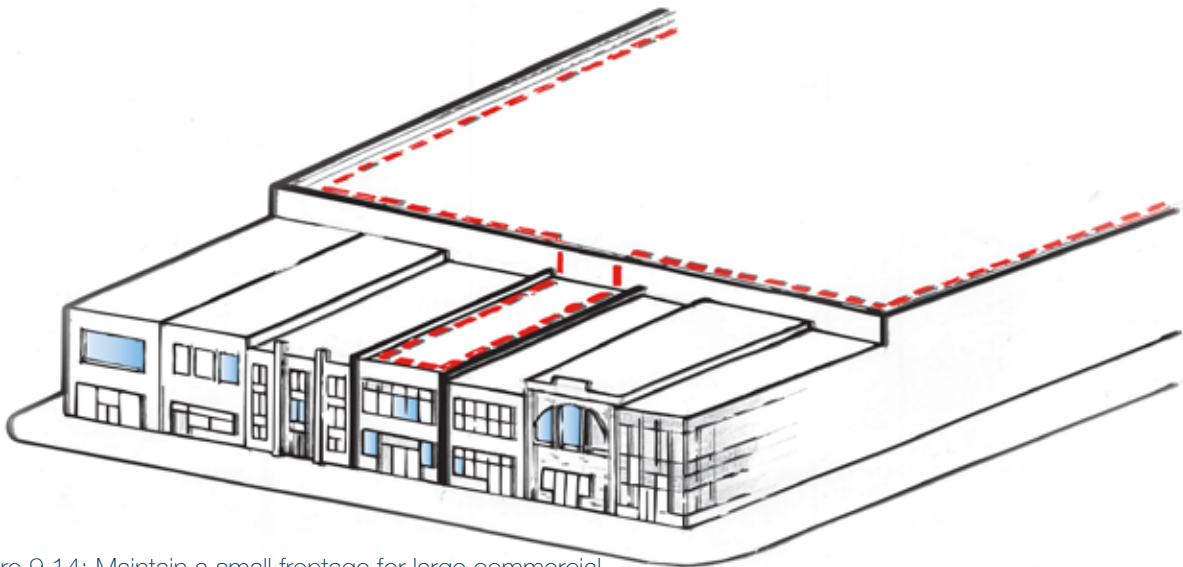


Figure 9.14: Maintain a small frontage for large commercial units

B. BUILDING MASSING AND DESIGN

9.2.9 Private Outdoor Amenity Space: Design shared outdoor amenity areas to be a focal point within the developments.

- i. Useable outdoor amenity spaces in the form of courtyards, forecourts, terraces, balconies, patios and rooftops (including podium surfaces) are strongly encouraged to enliven the private and semi-private realm.
- ii. Where private outdoor amenity space is provided for an individual units, the minimum area required is 6 m² for apartments and 9m² for townhomes, with 1.8 metres being the minimum for any one dimension.
- iii. For residential and mixed-use developments, accessible green rooftops and useable podium levels are encouraged to be provided as common amenity space. Consideration for indoor amenity space to be contiguous with outdoor amenity is strongly encouraged (See Figure 9.16).
- iv. Open spaces should be located and designed to maximize sunlight access during the day.
- v. Safety, comfort and the enjoyment of the amenity space should be enhanced by the provision of landscaping, seating, lighting, public art, and weather protection elements.



Figure 9.16: Encourage shared outdoor amenity space

B. BUILDING MASSING AND DESIGN

9.2.10 Building Separation and Setbacks, General:

Locate and design buildings to ensure sunlight and sky views are maximized while overlook conditions between buildings and neighbouring properties are minimized.

- i. Buildings should provide breaks in massing, offering visual relief in the urban streetwall and framing the street appropriately. Consider securing communal viewpoints through breaks in massing (See Figure 9.17).
- ii. Buildings should create a variety of spatial experiences at different scales to enhance the diverse quality of the area. Gardens, exterior courtyards and green spaces should be enhanced by building massing and siting.
- iii. Townhouse developments should provide breaks between buildings generally every 36 metres (based on groups of 6 units each 6m wide, or groups of 8 units each 4.5m wide) (See Figure 9.18).
- iv. Larger building masses (including width) or buildings with long frontages should generally not exceed 80 metres and are encouraged to provide visual breaks through shifts in massing, articulation, fenestration, recesses and/or other methods every 40 metres in order to maintain rhythmic variation along the street (See Figure 9.19).
- v. When buildings are required to stepback above the defined streetwall, a minimum building setback of 3 meters shall generally be applied (See Guidelines 9.2.6 viii., Map 9.2, and Figure 9.20).



Figure 9.17: Offer visual relief through building articulation



Figure 9.19: Provide visual breaks in rhythm and massing for long frontages



Figure 9.18: Provide breaks between buildings



Figure 9.20: Provide building stepback at the streetwall height, seen from above

B. BUILDING MASSING AND DESIGN

9.2.11 Building Separation and Setbacks, Tall Buildings:

Towers should be strategically located to minimize shadowing and adverse wind impacts on adjacent properties, parks and public spaces. Towers should also be sited to provide sufficient privacy between the building and adjacent properties. For the purposes of this Plan, tall buildings are defined as buildings 12 storeys or greater.

- i. Tower elements above the podium should have a minimum 20 metre separation with a 24.5 metre separation encouraged for residential towers (See Figure 9.21A).

- ii. Multiple towers planned on a single site should be off-set or weaved to accommodate view corridors, respect privacy and overlook and minimize negative impacts of shadowing.
- iii. Tower floor plates should generally not exceed:
 - Adjust floor plate maximums for residential and mixed-use buildings to 650 m² (7,000 sq.ft); and
 - Adjust floor plate maximum for commercial buildings to 836 m² (9,000 sq.ft).



Figure 9.21A: Desired building separation between residential towers

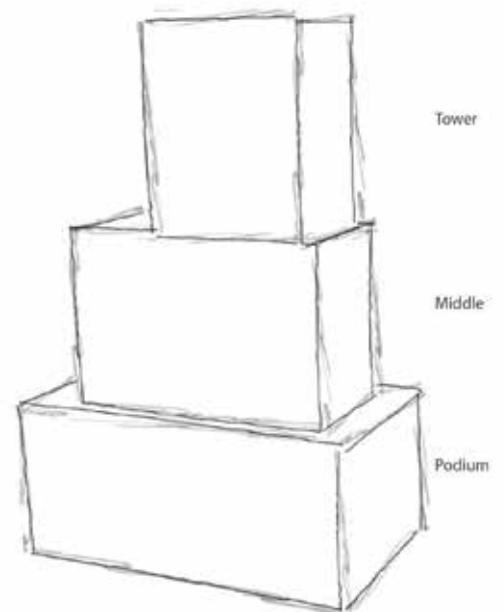


Figure 9.21B: Typical approach to built form for tall buildings

C. PUBLIC REALM

9.2.12 Streetscape, Landscape and Stormwater Management: Provide high-quality, sustainable spaces between buildings and adjacent streets, parks, plazas and other open spaces.

- i. Mixed-use and commercial developments should be sited to create a comfortable streetscape and include elements such as landscaped boulevards, stormwater management, weather protection features and articulated building facades.
- ii. Public open spaces, sidewalks, paths and connections should include quality permeable surface materials that produce, multi-purpose landscapes and support stormwater management best practices.
- iii. Development should integrate stormwater management best practices into the design of open spaces, including bioswales, flow-through planters and pervious strips.
- iv. Landscaping and new tree planting should contribute to an increased canopy cover and add resiliency to climate change impacts.
- v. The use of drought-tolerant native trees and vegetation as well as trees adapted to growing in heavily urbanized environments should be incorporated into open spaces.
- vi. Integrated stormwater management plans should include detailed assessments of the quality and quantity of stormwater and landscape plans that are aesthetic, functional and assist in managing on-site flows.



Figure 9.22: Integrate stormwater management into streetscape design



Figure 9.23: Create inviting, attractive and useable public spaces

9.2.13 Site Elements: Include well-designed site elements and ensure the proper placement of utilities to help elevate the quality and experience of the public realm.

- i. Plazas, urban squares and other public spaces should be designed to promote staying activities that humanize the environment and foster a sense of place (See Figures 9.23).
- ii. Lighting should enhance scenic qualities and night-time experiences of an area in addition to meeting functional requirements.
- iii. New developments should be designed to improve human comfort (e.g. microclimatic conditions) along streets and amenity areas.
- iv. The use of awnings, canopies, overhangs, colonnades, arcades and landscaping that provide respite and shelter from the elements is encouraged.
- v. Buildings should be designed to minimize the effects of shading and wind impacts on adjacent properties, public streets, public park spaces and other outdoor amenity spaces.

C. PUBLIC REALM

9.2.14 Building Elements: Support high quality architectural design and materials, appropriate building articulation and other thoughtful considerations in the building's design to promote diversity and visual interest.

- i. Architectural variation within development blocks is encouraged to reduce sameness in design. Design components should be complementary within the development as a whole.
- ii. Windows should be well-proportioned and provide relief, detail and visual rhythm on the façade while considering principles of passive design.
- iii. In commercial districts developments should provide a continuous weather protection system at grade. These systems should assist to articulate the base of the building and define the street edge (See Figure 9.24).
- iv. Roof structures should be designed to minimize the visual impact of rooftop equipment, using durable materials that are architecturally compatible with the building design (e.g. louvered screens).



Figure 9.24: Provide continuous weather protection to define street edges

9.2.15 Public Outdoor Amenity Space: Enhance the usability, comfort and appearance of outdoor amenity spaces within the public realm.

- i. Open spaces should include animated edges with active at-grade uses and be designed to be inviting, read as public space and encourage year round use.
- ii. Pedestrian circulation should be a focus of design and amenity spaces should be directly connected with the public street network and facilitate connections to active transportation routes, transit facilities, and community amenities and destinations.
- iii. Short-term bicycle parking should be located in highly visible, well-lit, accessible and weather protected areas.
- iv. Ensure public outdoor space includes a mix of hard and soft features, including surface treatments, trees, potted plants, flower beds, landscaped terraces and retaining walls.
- v. Public art is encouraged in public outdoor amenity spaces, including though integrated designs or as individual pieces to further humanize the built environment and animate the space.
- vi. Ensure that new public open spaces are designed to enable seamless pedestrian flows and provide connectivity to nearby destinations.
- vii. Outdoor amenity space associated with large scale commercial or institutional developments should be publicly accessible (See Figure 9.25).



Figure 9.25: Incorporate outdoor public amenity space into large scale commercial or institutional developments

10. SOCIAL AND CULTURAL WELL-BEING

Community health and well-being is strongly tied to the availability, accessibility and affordability of housing, food, community facilities, as well as programs and access to parks and green spaces. People of all ages, incomes, abilities, and backgrounds need access to basic services and ample opportunities for cultural and social interaction. Another key element of cultural well-being is understanding and acknowledging local history.

Several of the community values articulated for the Uptown–Douglas (UD) area directly relate to improving the social and cultural well-being of its residents and visitors. These values include having a safe place to live, work and explore, promoting active lifestyles, and building a sense of community for all abilities, ages and stages of life. Recognized internationally as an age-friendly community, the design of communities in Saanich places additional emphasis on urban environments that are accessible and welcoming to all.

Creating accessible public spaces, particularly those that provide services to a range of populations is a crucial element to community well-being. These spaces may host a range of resources, such as daycares, community kitchens, or simple outdoor gathering places. Ensuring a sense of safety along with a sense of social and cultural inclusion in these places is an important aspect of developing community.

The availability of housing that can serve the full spectrum of household needs is an essential component of a complete community. The UD area provides a diversity of housing, including a variety of non-market housing units with support services. Future development will play a critical role in expanding housing supply and diversity and ensuring new units are designed to foster social inclusion and connectivity to the neighbourhood. The Nigel Valley development will bolster the UD Plan area in this regard and include the renewal of valuable housing for low-income residents.

Art and culture play an important role in community well-being by providing opportunities for residents to interact and reflect on the place in which they live. The land has a rich indigenous history through the Songhees and Esquimalt First Nations and their connection to the land. More recently, buildings and trees have been identified for their heritage value and they provide important landmarks that help provide identity to the UD area. Art can also help to add a sense of place and identity to a community and can create a shared bond among residents. Saanich is committed to maintaining and growing its collection of public art in the District through its Comprehensive Arts Policy. The anticipated growth and changes to land use provides an opportunity for the Uptown-Douglas to become a central area of culture and art for the District of Saanich.

This Plan puts forth a bold vision to transform space to place. Through the pedestrian-oriented design of new buildings, the addition of parks and social gathering spaces and the emphasis on streets as public places, the area will become a much more active and vital community. This section supplements those fundamental directions and places an emphasis on fostering social inclusion and strengthening the area as a cultural destination through placemaking, supportive housing and the expansion of art and culture.

OBJECTIVES

- A. Encourage and promote active lifestyles through urban design that creates safe and comfortable walking and cycling options.
- B. Create a public realm that makes residents and visitors feel comfortable, safe and welcome.
- C. Facilitate and encourage the development of housing to accommodate residents of a variety of socio-economic statuses and stages of life.
- D. Ensure community facilities and programs are accessible and affordable for people of all ages, incomes, abilities and backgrounds.
- E. Honour and respect local First Nations' heritage and celebrate their unique art, history and culture.
- F. Acknowledge the history of the area and ensure cultural and heritage values are incorporated into planning and decision making.
- G. Promote and support public art and place-making initiatives that facilitate social interaction and create gathering spaces for a diverse population.
- H. Ensure that all residents have adequate and appropriate opportunities for involvement and engagement.
- I. Enhance opportunities for urban agriculture and improved access to local, healthy food.

10.1 SOCIAL WELL-BEING

A healthy community provides opportunities for people to nurture their physical, social and mental well-being. This includes access to suitable and affordable housing, nutritious food, employment opportunities, and opportunities for social inclusion. Public spaces are vital to social integration. These spaces should be inspiring, shared places, where people can engage in positive social interactions and encounters. A healthy community must ensure people feel safe and welcome, serve all members of the community at all stages of life, particularly those that are socially and/or economically marginalized.

The built environment should offer a variety of accessible public meeting places that bring residents and visitors together. Destinations such as parks, plazas, squares and playgrounds enable important social interaction between neighbours, while simultaneously serving as pleasant points

of connection along walking and cycling routes. Commercial environments such as cafés and restaurants are important “third” places, next to home and public spaces, as are workplaces, schools and venues for culture, recreation and sport. Access to sports facilities and green spaces, and opportunities to enjoy physical outdoor pursuits are important in maintaining a physically active population.

A healthy neighbourhood includes services such as a school, daycares located near schools and employment centres, and healthcare facilities. Locating community services in easily accessible areas of the Uptown-Douglas (UD) Plan area, and throughout the District, will ensure that all residents have an opportunity to take advantage of programs, facilities and amenities that promote health and well-being.

POLICIES

- | | |
|--------|--|
| 10.1.1 | Support the development of social service facilities that are responsive to the needs of vulnerable members of the Saanich community. |
| 10.1.2 | Promote healthy living in the UD area by supporting active lifestyles through the addition of new parks, plazas and active recreation opportunities and enhancements to create a safe and inviting public realm. |
| 10.1.3 | Invest in enhancements to and support the development of new public parks, playgrounds, daycare centres, community centres, schools, libraries, and other essential services needed to attract and sustain families locating in the Uptown-Douglas area. |
| 10.1.4 | Encourage the location of new childcare centres in convenient pick-up and drop-off locations (i.e. along major transit and commercial corridors, employment hubs, high density neighbourhood sub-areas). |
| 10.1.5 | Develop new, and enhance and retain existing, public and private spaces for cultural and social activities, emphasising flexible uses that can evolve over time. |
| 10.1.6 | In partnership with community service providers, ensure that high-quality, neighbourhood-specific programs and services for children and youth are accessible, affordable and culturally appropriate. |
| 10.1.7 | Ensure residents are kept apprised of proposed changes in the community and have ample opportunities to be informed and share their opinions. |
| 10.1.8 | Improve the physical connection of the Municipal Campus site within the community and promote it as a place where residents can engage in civic discourse. |
| 10.1.9 | Encourage housing that supports a broad range of accessibility and age-friendly features, preferences, and family structures, including the ability to age in place (see Policies 5.2.2, 5.2.9 and 5.2.10). |

10.2 SUPPORTIVE AND AFFORDABLE HOUSING

Having suitable and affordable housing plays an important role not only in a community's quality of life, but also in its economy, health, and sustainability. Housing for low to moderate income households is in notably high demand in the District of Saanich and the Capital Region. Saanich undertook a Housing Needs Report in 2020. The report shows a shortage of housing for "very low" and "low to moderate" income levels. The need is particularly acute for renters. In 2021 Saanich completed a Housing Strategy to address challenges in the availability and cost of housing in Saanich within the next ten years. Housing that is adaptable, age friendly, coupled with support services or otherwise considers the unique needs of a diverse population is crucial

to community well-being. Such diverse and affordable housing forms enable all residents, specifically those that are marginalised or isolated, the means to remain a part of the community.

The growth and land use change anticipated for the Uptown–Douglas (UD) area provides an opportunity for the District of Saanich to be a leader in providing inclusive and community oriented housing options. The land use policies that support the addition of diverse and affordable housing are outlined in Section 5.2. This section provides additional policies aimed at building a housing supply in the UD area that encourages social inclusion and helps to meet the housing demands of the District and Region.

POLICIES

- | | |
|--------|---|
| 10.2.1 | Support the development and maintenance of affordable and supportive housing throughout the UD area. |
| 10.2.2 | Undertake and regularly update a Saanich Housing Needs Assessment and use the results to guide priorities around the types of housing units that are targeted for redevelopments in the UD. |
| 10.2.3 | Retain existing purpose built rental units or support the integration of the same or a greater number of rental units as a component of new developments. |
| 10.2.4 | Support and incentivize non-market housing developments, or developments with an affordable housing component (See also Policy 5.2.3), and further encourage such developments to locate in close proximity to transit, services and amenities. |
| 10.2.5 | Support the development of housing and associated community service facilities for marginalized populations, people with special needs and seniors, and strive to locate such developments in close proximity to transit, services and amenities. |
| 10.2.6 | Encourage the use of the Voluntary Design Guidelines for Adaptable Housing when considering the inclusion and design of Adaptable Dwelling Units (as defined in the British Columbia Building Code, as amended) in new developments. |
| 10.2.7 | Work with the Capital Regional District, BC Housing, Canada Mortgage and Housing Corporation, and other Provincial agencies to support the renewal/redevelopment of existing non-market housing to that would result in reduced GHG and sustain greater efficiencies and reduce operating costs. |
| 10.2.8 | Expect new residential developments to provide 2 and 3-bedroom units appropriate for families, and encourage additional child-friendly features suitable to supporting family and needs, and include: <ul style="list-style-type: none">• ground floor units with accessibility features;• bedroom sizes that accommodate a queen size bed;• additional in-suite or on-site storage space;• private outdoor space (see Policy 5.2.7); and• amenity and/or common spaces that incorporate design elements to encourage social connections for residents, including families, children and adolescents, such as play space and playgrounds. |

- 10.2.9 Encourage the use of energy reduction features in non-market and market rental developments, including Step Code building energy performance beyond regulated levels, to reduce operating and utility costs.
- 10.2.10 Support the development of non-market and affordable rental housing through considering:
- reductions of municipal fees such as waivers to DCCs;
 - parking variances; and
 - continued prioritized application review and approval timelines.
- 10.2.11 Promote quality design and innovation for new affordable housing developments, with a focus on sustainable, energy conserving, and attractive design that will reduce maintenance costs and increase neighbourhood acceptability.
- 10.2.12 Collaborate with social service providers to identify opportunities for supportive housing, transition housing, and subsidized housing to help reduce the paths to homelessness.
- 10.2.13 Continue to engage with the community on housing affordability issues and communicate the importance of supportive housing and community service facilities.
- 10.2.14 Support implementation of the approved Nigel Valley development concept as a key aspect of creating a diverse housing stock in the UD area and strive to integrate the development seamlessly with the surrounding area (See also Policy 5.12.8).
- 10.2.15 Support implementation of the Housing Strategy to address housing priorities in the Uptown-Douglas area.



Vergo - Affordable Housing, Capital Regional District



Olympic Visa - Affordable Seniors Housing, Cool Aid Society

10.3 CULTURE

Saanich residents and visitors spend much of their time in parks, squares and other public spaces, including District buildings and facilities. As the Uptown–Douglas (UD) area grows and changes, new development and redevelopment should create space for cultural expression in various forms. Publicly visible and accessible works of art help to establish multi-faceted, interesting, attractive and democratic public spaces. Diverse cultural programs, activities and opportunities for engagement contribute to a thriving community. Public and private spaces can be designed to reflect or incorporate a range of cultural traditions, fostering community interactions and creating a sense of belonging.

The creation of cultural places should acknowledge and embrace the rich indigenous history of the land and be representative of the traditions of the Songhees Nation and Esquimalt Nation. As noted in Section 2.2, Saanich has extensive indigenous history and there are opportunities to celebrate and showcase the historical and cultural connections. Continuing to learn, explore and celebrate these connections will contribute to the creation of a meaningful, vibrant and socially inclusive place.

POLICIES

- 10.3.1 Work with the local First Nations, notably the Esquimalt and Songhees Nations, to better understand the pre-colonial conditions and cultural practices and explore opportunities to acknowledge and celebrate their history and culture.
- 10.3.2 Support cultural events and programming that can animate the area.
- 10.3.3 Explore the introduction of cultural facilities in the UD area, including through the redevelopment of larger sites or as the component of new park spaces.
- 10.3.4 Expect public art projects in key strategic locations, including those identified on Map 10.1, and continue to explore the coordination, funding and/or implementation of these pieces and projects.
- 10.3.5 Support efforts to integrate local First Nations art within the UD area.
- 10.3.6 Integrate public art into the public realm, enhancing the pedestrian experience, complementing the surrounding built environment and raising awareness of the history of the area.
- 10.3.7 Ensure spaces for cultural and social activity reflect the character and identity of the neighbourhood.
- 10.3.8 Consider cultural programming needs when designing public and private spaces.



Public art in Rutledge Park



A

- COMMUNITY FACILITIES / POINTS OF INTEREST

142

10.4 HERITAGE SITES

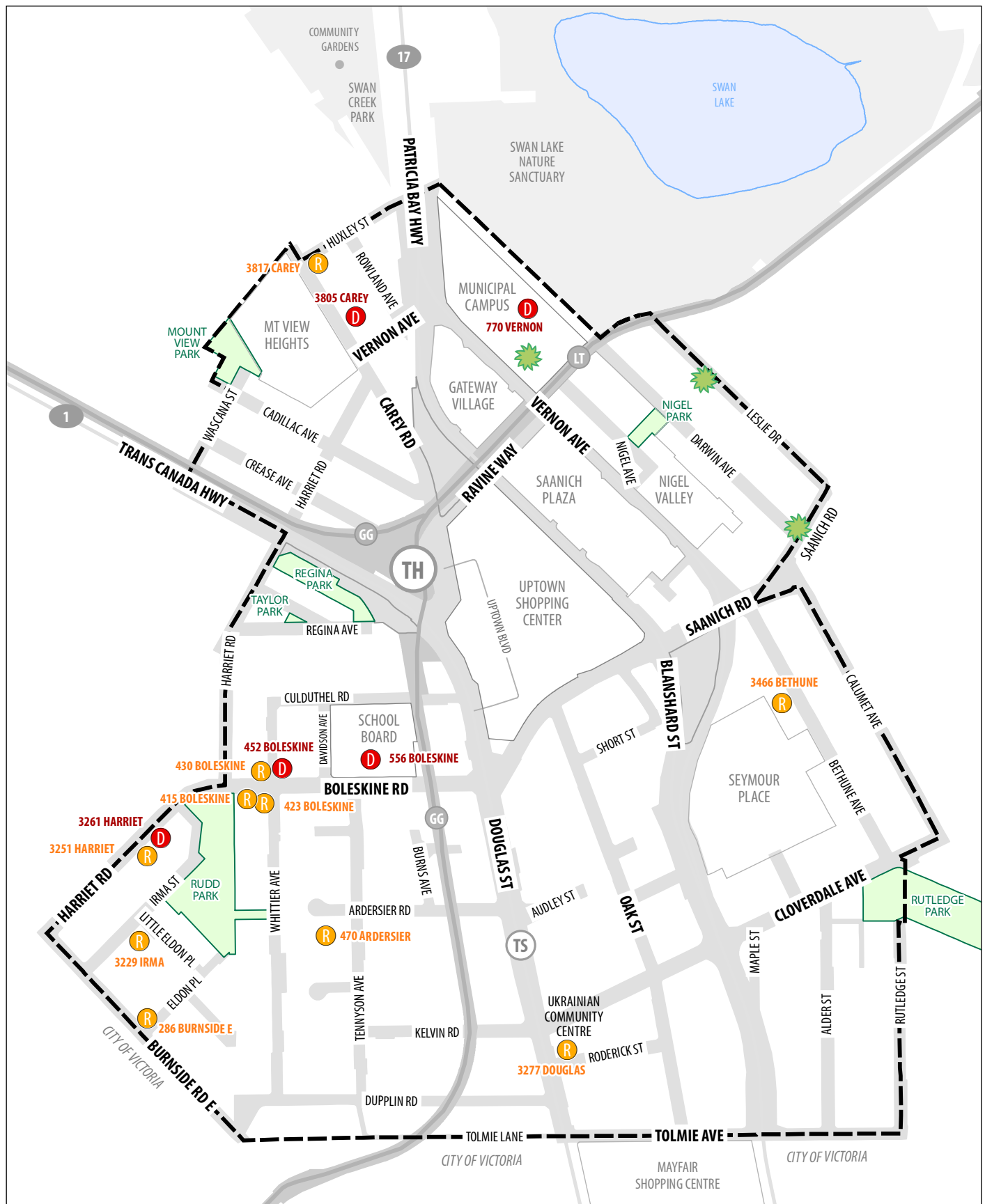
An important part of the identity of a community is its natural and built heritage. A number of features in the Uptown–Douglas (UD) area add to the character of the area. Saanich, and particularly the commercial node within the Plan boundaries, experienced a land boom in the period between 1910 and 1914. This period of construction is reflected by heritage buildings in the area. Significant buildings that are prominent on the UD area landscape include Tolmie School, built in 1913, and Saanich Municipal Hall, completed in 1965. The District of Saanich works with other organizations to protect its heritage buildings and sites. In addition to

preserving heritage in the built form, Saanich strives to protect trees that are unique or historically significant.

Over a dozen buildings of historical significance, predominately built in the early twentieth century, still scatter the landscape. Heritage Assets are shown on Map 10.2. Figure 10.1 identifies buildings of historical significance as listed in the Saanich Heritage Register, including five buildings with a Heritage Designation, as well as three significant trees (as per the Tree Protection Bylaw).

POLICIES

- 10.4.1 Recognize and celebrate the value of culturally diverse heritage assets in the UD area and Saanich as a whole.
- 10.4.2 Preserve and enhance the public visibility of all properties on the Saanich Heritage Register, with special consideration to Heritage Designated properties and buildings.
- 10.4.3 Ensure preservation, rehabilitation and restoration of heritage assets through any proposed redevelopment.
- 10.4.4 Encourage owners to seek Heritage Designation when considering proposed redevelopment of properties on the Saanich Heritage Register.
- 10.4.5 Promote design sensitivity when considering rezoning, development and subdivision applications in the vicinity of buildings or sites on the Heritage Register or with a Heritage Designation, or in the vicinity of significant trees.
- 10.4.6 Maintain streetscape views and a strong visual presence of heritage assets.
- 10.4.7 Continue to support the Saanich Heritage Foundation and the Significant Tree Committee in their respective roles in preserving the District's heritage assets.
- 10.4.8 Support a comprehensive development project that preserves, restores and enhances the Tolmie School Building, a designated heritage site located at 556 Boleskine Road, while encouraging new development to be integrated on the site.
- 10.4.9 Seek to include sites of heritage significance as part of a wayfinding framework and promote heritage in the area (e.g. walking tours).
- 10.4.10 Explore potential additions to the Heritage Register, including 520 Ardersier Road.



UPTOWN-DOUGLAS
PLAN



HERITAGE
DESIGNATED



HERITAGE
REGISTERED



SIGNIFICANT
TREES

Heritage Buildings

Designated

452 Boleskine Road	Graham Residence	(1913)
556 Boleskine Road	Tolmie School	(1913)
3805 Carey Road	Mountain View Residence	(1914)
3261 Harriet Road	Johnson Residence	(1913)
770 Vernon Avenue	Saanich Municipal Hall	(1965)

Registered

470 Ardersier Road	Home Lumber Office	(1972)
3466 Bethune Avenue	Leahy Residence	(1912)
415 Boleskine Road	Tapscott Residence	(1914)
423 Boleskine Road	Crowther Residence	(1911)
430 Boleskine Road	Axhome Residence	(1899)
286 Burnside Road East	Hancock Residence	(1905)
3571 Calumet Avenue	McAdoo Residence	(1919)
3817 Carey Road	Henson Residence	(1937)
3277 Douglas Street	Douglas Street Baptist Church	(1913)
3251 Harriet Road	Stoddart Residence	(1895)
3229 Irma Street	Matthews Residence	(1895)

Significant Trees

833 Leslie Drive	Black Locust	(#174)
3666 Saanich Road	Garry Oak	(#84)
770 Vernon Avenue	Contorted Pekin Willow	(#16)

Figure 10.1 Identified Heritage Assets



Rudd Park, 1946.



St. Marks Church, 1893.



Axhorne Residence, 430 Boleskine Road.



Johnson Residence, 3261 Harriet Road.



Tapscott Residence, 415 Boleskine Road.



Crowther Residence, 423 Boleskine Road.



McAdoo Residence, 3571 Calumet Avenue.



Henson Residence, 3817 Carey Road.

10.5 AGRICULTURE AND FOOD SECURITY

The District of Saanich recognizes the importance of agriculture and food security as part of a sustainable community. Currently, the majority of food consumed in our region is imported which increases Saanich's vulnerability to market changes. Interest in food production in urban areas has expanded dramatically in recent years and is seen as a key way to supplement conventional agriculture and bring community members in touch with local food and natural processes.

The District of Saanich's Official Community Plan and Agriculture and Food Security Plan prioritize expanding opportunities for food production in urban areas. Urban agriculture is encouraged in order to reduce "distance to plate",

encourage social interaction, and allow more locally grown food production. While the Uptown-Douglas (UD) area is within the Urban Containment Boundary, agriculture and food security can play an important role in meeting the area's social, environmental, economic, and health goals.

Saanich protects its high capability agricultural land, but despite this access to healthy, local food is difficult for some households. Approximately 6% of households in Greater Victoria are considered 'food insecure'. Directions in the Plan contribute to the goal of raising production levels of food on the local land base and also address convenient access to healthy food by residents.

POLICIES

- 10.5.1 Incorporate fruit and nut bearing trees and other edible landscaping elements on suitable public and private lands, where appropriate.
- 10.5.2 Encourage access to healthy and culturally appropriate food by supporting farmers markets, community gardens, community meals, food support programs and similar initiatives.
- 10.5.3 Consider community gardens within existing or proposed parks, undeveloped parcels, and closed right of ways, as per the District of Saanich Community Gardens Policy.
- 10.5.4 Recognize, support and enhance existing community food security initiatives, including through partnerships with the School Board and other institutional partners.
- 10.5.5 Support community food hubs and their work in redirecting food resources that would otherwise be disposed.
- 10.5.6 Support the establishment of uses that expand access to and awareness of local food and animate the area, including farmers markets, on-site farm stands and food trucks.
- 10.5.7 Support the introduction of facilities in industrial areas that support local food production, distribution and access.
- 10.5.8 Continue to utilize the Municipal Campus as a location to showcase food production and food accessibility.
- 10.5.9 Integrate spaces for food production within commercial and multi-unit developments, both at grade and on shared / accessible rooftops.
- 10.5.10 Encourage community contributions that introduce assets that enhance food security and support agriculture.



11. ECONOMIC VIBRANCY

A vibrant and sustainable economy is one that provides diverse and viable economic opportunities that meet the needs of present and future generations – it is an important feature of a prosperous community. The Uptown-Douglas (UD) area has close to 10,000 employment opportunities, a diversity of shops and services, and a developing housing stock. With regionally significant transportation corridors and proximity to downtown Victoria, it is a true economic centre in the Capital Region. With continued development in the Westshore and downtown Victoria, and the planned expansion of high frequency transit lines, the area is well positioned to be an economic hub for the region into the future.

Significant economic advantages, including commercial, industrial and transportation assets will be built upon to enhance and diversify the area. Numerous retail and commercial outlets such as the Uptown Shopping Centre, which continues to develop, the Saanich Plaza and the Gateway Plaza will serve as a foundation for economic growth. The UD area's future as a dense, mixed use community offers an opportunity to enhance these existing assets and create a sustainable, prominent regional hub, with a vibrant and diverse local economy. Additionally, new infrastructure to support quality of life and livability, such as parks, daycares, social gathering spaces and shops will help businesses attract and retain employees.

The area also has a strong industrial presence with 30% of Saanich's industrial lands. Industrial lands are important to both the local and regional economy, providing employment opportunities and essential services. In recent years, industrial vacancy rates have been declining and the demand for industrial land is growing, signaling a need for intensification. Additionally, the types of industrial spaces required have been shifting with trends in the larger economy, resulting in new space requirements for emerging sectors. The UD area has an opportunity to meet the needs of a changing economy while preserving industrial uses and creating a dynamic, integrated neighbourhood. Being at the forefront of an emerging economy offers an opportunity to grow the UD in an economically, socially, and environmentally sustainable way.

Economic vitality in the UD area will be strengthened and supported through the realization of the land use intensification and transit supportive development directions found throughout this Plan. This section focuses specifically on economic growth and diversification, and how appropriate supports can be provided for entrepreneurs and businesses of all sizes and types to enable them to continue to thrive.

OBJECTIVES

- A. Become a preeminent regional center for commerce, employment, arts, culture and entertainment.
- B. Support and encourage a diversity of commercial uses, including both regional and local serving retail, office, technology oriented, institutional and visitor accommodation uses.
- C. Enhance public realm conditions to create a leading location in the region for business and commercial activity and an attractive location for new development.
- D. Allow more flexible uses in industrial areas to accommodate the changing needs of this sector.
- E. Foster innovative business and urban development by supporting emerging industries and associated education opportunities.
- F. Support greater access to and expansion of economic opportunities for all in order to achieve an equitable allocation of the benefits of development and economic growth.

10.1 ECONOMIC ENHANCEMENT AND DIVERSIFICATION

This Plan aims to retain and increase employment within the Uptown-Douglas (UD) area to enhance its status as a key economic centre in the region. Employment retention and growth will be achieved by providing opportunities for intensification and diversification of commercial and industrial lands to support a growing variety of business needs

Commercial intensification will help to create a hub of retail and services. Diversification emphasizes the importance of retaining industrial lands while recognizing the growing shift away from traditional industrial forms towards lighter, multi-purpose forms, including the tech industry.

The policies in this section seek to promote economic enhancement and diversification in the UD area and three Neighbourhood Sub-areas (the Douglas - Oak Hub, Tolmie Quarter and the Tennyson Industrial Quarter).

GENERAL POLICIES

- 11.1.1 Foster the UD as an employment centre for office, professional services and retail by supporting existing and new development and attracting anchor tenants, with a focus on areas identified as Core in Map 5.1.
- 11.1.2 Strengthen the UD as a location for job creation by supporting a variety of commercial uses that attract both regional and local serving retail.
- 11.1.3 Support efforts to make the UD area a competitive and affordable location for development and business location and operation.
- 11.1.4 Explore the introduction of cultural facilities in the UD area and encourage physical links to related businesses such as restaurants, cafes, and visitor accommodations (See also Policy 10.3.3).
- 11.1.5 Support greater access to and expansion of education, housing and employment opportunities for groups facing longstanding disparities to ensure an equitable allocation of the benefits of development and economic prosperity.
- 11.1.6 Support the development of an Economic Development Strategy, on a District or Regional scale, to support the ongoing implementation of the Uptown-Douglas Plan, and include:
 - Analysis for existing and future employment lands.
 - Development of an Industrial Land Strategy.
 - Assessment of Uptown-Douglas objectives and policies to ensure they align with the economic development vision.
 - Consideration of Saanich-specific solutions, with a focus on looking at the balance of employment and housing (“Made in Saanich” solution).

NEIGHBOURHOOD SUB-AREA POLICIES

Douglas - Oak Hub

- 11.1.6 Support development applications along Douglas Street that accommodate a strong concentration of employment and commercial uses along with complementary uses such as multi-unit residential development, hotels, restaurants, public institutions, personal service businesses and retail stores.
- 11.1.7 Support development along Oak Street and Audley Crossing that includes new multi-unit buildings and more pedestrian-oriented commercial including retail, restaurants, cafes, and entertainment.

Tennyson Industrial Quarter

- 11.1.8 Support density increases that provide additional employment opportunities and protect the industrial character of the area (See also Policy 5.8.1).
- 11.1.9 Encourage a range of businesses to locate in the Tennyson Industrial Quarter, from start-up firms to corporate headquarters, with particular focus on knowledge-based industries such as technology, research and development.
- 11.1.10 Support the growth of new industrial sectors and promote this sub-area as a place where startups can transition to mature businesses.

Tolmie Quarter

- 11.1.11 Support live-work units, preferably fronting the Galloping Goose Regional Trail or Boleskine Road, as a means to support small, local businesses.
- 11.1.12 Promote mixed-employment as a means to establish the area as a centre of learning and community services.

11.2 ECONOMIC SUSTAINABILITY

The Uptown–Douglas (UD) area is an economic hub for the Capital Region, envisioned to remain robust and resilient into the future. One way of achieving resiliency is through creating and maintaining a diversity of economic assets and, as articulated in the Community Values section of this Plan, supporting economic investment and development that fosters a range of business and employment opportunities. A diversity of assets will be realized through supporting local economic

development, providing services for employees in the area, providing opportunities for new businesses to establish and thrive, and providing diverse transportation options for employees to commute to and from the UD area.

The policies in this section aim to establish and enhance a diversity of economic assets in the UD area and two Neighbourhood Sub-areas (the Douglas - Oak Hub and the Tennyson Industrial Quarter).

GENERAL POLICIES

- 11.2.1 Support the development of a Business Improvement Association to provide business support and marketing to business owners, help navigate business and development processes, and build a collective identity for the UD area.
- 11.2.2 Ensure job space is well integrated with the transportation network by providing greater proportions of office and other higher ridership uses in proximity to transit stations and the Galloping Goose Regional and Lochside Regional Trails.
- 11.2.3 Engage businesses in providing daycare facilities on-site to attract and retain a diverse workforce.
- 11.2.4 Encourage “high performance areas” that promote energy efficient and low carbon building design and practices, particularly in areas with large amounts of planned new development.
- 11.2.5 Encourage flexible building design and construction that allows buildings to be repurposed and accommodate a variety of uses in the future.



SUB-AREA POLICIES

Douglas - Oak Hub

- 11.2.6 Enhance the vibrancy of the Douglas Street corridor to optimize its potential to attract investment and the development of new commercial uses.

Tennyson Industrial Quarter

- 11.2.7 Promote the Tennyson Industrial Quarter as a center for innovative business and development practices, fostering job creation in sustainable industries.
- 11.2.8 Foster the long term success of industrial businesses while supporting their evolution into places with a broader mix of businesses, living-wage jobs, and higher employment densities.
- 11.2.9 Improve the urban design and character of streets in the area to increase its attractiveness to existing and future employers.

Tolmie Quarter

- 11.2.10 Encourage a Master Plan for the redevelopment of the School Board site, ensuring a commitments towards the preservation of its heritage assets and supply of new community facilities.



12. TAKING ACTION AND TRACKING PROGRESS

12.1 PRIORITY ACTIONS

The implementation of the Uptown–Douglas (UD) Plan will require sustained actions by the public and private sectors over the course of the 20-30 year time horizon of the Plan. While a primary role of this Plan is to provide a framework to guide and evaluate change, this section highlights a number of priority actions that will be undertaken to realize the goals and objectives of the Plan as a whole. These actions mostly relate to items that are within or partially within the control of the District of Saanich.

The policies in this Plan provide clarity with respect to future objectives in the area, providing a level of certainty to guide investment and evaluate decision points that emerge. Many of these policies specifically related to development processes and associated negotiations.

The following table (Figure 12.1) outlines policy actions and their associated priority levels and assigns leadership to the appropriate department(s) within the District. As determined through the District's annual strategic planning and budget processes actions will be undertaken systematically to implement priority actions of this Plan.

Figure 12.1 Priority Actions

Policy Actions	Policy No.	Lead Department(s)	Priority
Environment and Sustainability			
Explore incentive tools and programs to encourage development of net zero carbon and/or net zero energy ready buildings.	4.2.3	Planning	High
Undertake a feasibility study to assess the potential of developing a district energy system designed for renewable fuels in the UD area.	4.2.5	Planning + Engineering	Low
Support the Capital Regional District in undertaking and inter-municipal study to comprehensively assess the Cecelia Cree Watershed, assess feasibility of daylighting Cecelia Creek and identify priority actions for improvements.	4.4.1	Parks + Planning	Medium
Explore opportunities to enhance the ecological value of Regina Park with respect to stormwater management, biodiversity and pollutant mitigation (see also Policy 8.3.7).	4.5.3	Planning + Parks	Medium
Prepare an inventory of supported planting materials for development projects that reflect the principles of Naturescape and includes drought-resistance and native species.	4.5.5	Planning + Parks	Medium
Land Use			
Undertake an analysis to enable the development of a structured amenity policy and contribution rate for rezoning applications that would establish a transparent and predictable system for obtaining community amenity contributions in the UD Plan area.	5.6.5	Planning	High
Pursue the development of pocket parks or plazas along the Galloping Goose Regional Trail, including at the intersection with Dupplin Road (See also Policy 8.3.3).	5.8.6	Planning, Parks + Finance	Medium
Work with the Ministry of Transportation and Infrastructure to explore potential options to utilize the highway island on Blanshard Street for public benefit.	5.10.6	Planning, Parks + Engineering	Low

Figure 12.1 Priority Actions

Policy Actions	Policy No.	Lead Department(s)	Priority
Transportation and Mobility			
Acquire connections for walking and cycling, as identified on Map 6.2 to achieve overall network connectivity.	6.1.1	Planning, Engineering + Parks	Medium
Establish an alternative transportation reserve fund for the UD area specifically for developing pedestrian and bicycle facilities.	6.2.7	Planning + Engineering	Low
Review and update parking requirements in the Zoning bylaw with the intent to lower parking requirements in the UD area, immediately following the adoption of this Plan.	6.6.4	Planning	High
Prepare an inventory of existing parking restrictions and utilization rates to inform development of a parking management strategy.	6.6.7	Planning + Engineering	High
Develop a Memorandum of Understanding (MOU) with the MOTI which identifies agreed upon design and development approaches that reflect the UD context and policy directions and provides predictability for future capital projects and improvements required as part of re-development proposals.	7.1.3	Planning + Engineering	High
Explore options for Saanich to gain control of MOTI owned roads for the purpose of implementing complete street designs on urban arterial roads.	7.1.4	Planning + Engineering	Medium
Significant Streets			
Develop a Memorandum of Understanding with the Ministry of Transportation and Infrastructure (MOTI) which identifies agreed upon design and development approaches for Significant Streets that reflect the UD area context and policy directions.	7.1.3	Planning + Engineering	High
Work with the Ministry of Transportation and Infrastructure (MOTI) to analyze feasibility of reducing the one-way section of Blanshard Street to three lanes.	7.3.2	Planning + Engineering	High
Explore adding a new connection to the Lochside Regional Trail from Ravine Way.	7.5.3	Planning, Parks + Engineering	Medium
In collaboration with the business community, support temporary closures of Oak Street for festivals or other community celebrations.	7.8.7	Planning, Parks + Engineering	Ongoing

Figure 12.1 Priority Actions

Policy Actions	Policy No.	Lead Department(s)	Priority
Parks, Open Spaces, Trails and Community Facilities			
Acquire new community park spaces through the DCC program.	8.1.2 + 8.1.3	Planning + Parks	High
Work with the Capital Regional District to enhance the Galloping Goose Regional and Lochside Regional Trails.	8.3.1	Planning + Parks	Ongoing
Enhance the connectivity to the Lochside Regional Trail and pursue the addition of a public /open space at the Municipal Campus site that complements the Nigel Valley Plaza through upgrades or redevelopment opportunities.	8.3.7	Planning, Parks + Engineering	Medium
Review recreational programming and facilities in the context of projected population growth for the UD to ensure they are meeting emerging needs.	8.4.1	Planning + Parks	Low
Work with the School Board to accommodate long-term needs for school facilities in the area.	8.4.6	Planning + Parks	High
Explore the introduction of youth oriented facilities and amenities in Rudd Park.	8.4.9	Parks	Medium
Urban Design			
Integrate the Urban Design Principles and Framework into the development of updated Development Permit Area Guidelines.	9.1.1	Planning	High
Develop a wayfinding strategy for the UD area.	9.1.10	Planning + Engineering	High
Social and Cultural Well-Being			
Undertake and regularly update a Saanich Housing Needs Assessment.	10.2.2	Planning	High
Explore the introduction of cultural facilities in the UD, including through the redevelopment of larger sites or as the component of new park spaces.	10.3.3	Planning	High
Include sites of heritage significance as part of a wayfinding framework and promote heritage walking tours in the area.	10.4.9	Planning	Medium
Economic Vibrancy			
Support the development of an Economic Development Strategy, on a District or Regional scale, to support the ongoing implementation of the Uptown-Douglas Plan.	11.1.6	Planning	Medium
Support the development of a Business Improvement Association for the UD area.	11.2.1	Planning	Medium

12.2 MONITORING AND EVALUATION

It will be important to monitor the progress of the Uptown-Douglas (UD) Plan in order to assess its implementation and make sure that it continues to be an effective tool for guiding land use and planning decisions and meeting community needs.

At five year intervals, an evaluation of the progress of the UD Plan area will be undertaken to assess the following:

- progress of Key Plan Directions;
- updating demographic, housing and household census data;
- alignment with other policy documents and initiatives; and
- other indicators as necessary.

The result of the evaluation could result in Plan amendments, identification of future study, revisions to the Plan, and additional actions. The findings will be provided in a report to Council for their consideration and direction.



13. APPENDICIES

13.1 GLOSSARY

Accessibility

The degree to which a product, service or environment is available to as many people as possible. The concept often focusses on barrier-free design – designs intended to assist those with a particular limitation (e.g. people with disabilities or special needs).

Affordable Housing

Housing where the rent or mortgage plus taxes is 30% or less of a household's gross annual income. Households that have no option but to pay more than this percentage of their gross income on shelter expenditures, in reasonable condition and of appropriate size, are households that are in need of affordable housing.

All Ages and Abilities (AAA)

A network of interconnected bicycle facilities that are both comfortable and attractive to all users, regardless of ability, and designed to be suitable for persons aged 8 to 80 years old. Typical 'AAA' facilities include bicycle boulevards, protected bicycle lanes and multi-use pathways.

Amenities

Items that add to the physical, aesthetic, and/or functional appeal of a particular site, neighbourhood or the community in general.

Active Commercial Use

Commercial units, typically located at grade or the first two storeys of a development, and include services that generate activity within the public realm (e.g. cafes, restaurants, retail shops, service shops, etc.) through opportunity for spill-over of uses and often combined with increased transparency of store fronts.

Biodiversity

All varieties of life and their processes, encompassing the full range of natural variability, including genetic diversity, species diversity, and ecosystem diversity.

Car Share or Car Co-op

A system where a fleet of cars is made available for use by members of the car share group in a wide variety of ways.

Climate Change

Any long-term significant change in the "average weather" that a given region experiences. Average weather may include average temperature, precipitation and wind patterns. It involves changes in the variability or average state of the atmosphere over durations ranging from decades to millions of years. These changes can be caused by dynamic process on Earth (ocean processes, volcanoes), external forces including variations in sunlight intensity, and more recently by human activities.

Community Amenity Contributions (CACs)

Community Amenity Contributions (CACs) are amenity contributions offered by developers and agreed to by local governments, often but not exclusively as part of a rezoning process initiated by the developer. CACs typically include the provision of amenities, affordable housing, or financial contributions towards amenities, or some combination of these.

Complete Streets

Roads designed and operated to enable safe access for all users, including pedestrians, bicyclists, motorists and transit riders of all ages and abilities. Complete streets will look different based on the context and location, but may include wide sidewalks, protected bike lanes, designated transit lanes, comfortable and accessible transit stops, frequent and safe crossing opportunities, accessible pedestrian signals, landscaping, intermittent shelter, or narrower travel lanes. Complete streets promote a shift in the way road design is approached, both in terms of designing for the full variety of users, and in terms of ensuring safe and reliable integration with the larger transportation network.

Crime Prevention Through Environmental Design (CPTED)

A multi-disciplinary approach for reducing crime through urban and environmental design and the management and use of the built environment.

Daylighting

Restoring a watercourse that has been channelized and or contained within a pipe or manmade structure, to its natural state.

Density

As defined in the "*Local Government Act*" S. 872: "the density of use of the land, parcel or area, or the density of use of any buildings and other structures located on the land or parcel, or in the area".

Development Cost Charge

A levy applied to new development to offset the long-term cost of providing new or extended services to the community. For example, development cost charges are imposed under Saanich's Development Cost Charge Bylaw, 2019, No. 9553, as amended or replaced from time to time.

Development Permit Area

An area designated pursuant to the *Local Government Act* where approval of a development permit is required before a building permit can be issued or a subdivision is approved with specified exemptions. Development Permit Areas may be established to: protect the natural environment and bio-diversity; protect development from hazardous conditions; revitalize designated commercial areas; guide the form and character of commercial, industrial, and multi-unit development; and guide the form and character of intensive residential development or to protect farming.

Dwelling Unit

A self-contained set of habitable rooms with a separate entrance intended for year-round occupancy with complete living facilities for one or more persons, including provisions for living, sleeping, cooking, and sanitation.

Ecosystem

A complete system of living organisms interacting with the soil, land, water, and nutrients that make up their environment. An ecosystem is the home of living things, including humans. It can be any size, but it always functions as a whole unit. Ecosystems are commonly described according to the major type of vegetation, for example, an old-growth forest or a grassland ecosystem.

Floor Space Ratio (FSR)

The figure obtained when the total floor area of all floors in all buildings on a parcel is divided by the area of the parcel.

Green Building

A systems approach to a building's design and construction that uses techniques to minimize environmental impacts and reduce ongoing energy consumption while contributing to the health and productivity of its occupants.

Green Infrastructure

At a neighbourhood or site specific level, it is an approach to managing run-off and rainwater that imitates the natural hydrology (or movement of water) on-site by using existing site characteristics and engineered and landscaped features to promote infiltration and evapotranspiration.

Green Roof

A landscape on a roof which can be distinguished between two major types, namely intensive and extensive roofs. Intensive green roofs can be seen as a gardening and living space, going beyond functional utility by additionally providing aesthetical and social value. In contrast, extensive roofs are kept simple and seen solely as a functional unit for achieving cost savings. In general, this Plan seeks application of intensive green roofs as an extension of open spaces (private or public).

Greenhouse Gas (GHG)

Gases present in the atmosphere which reduce the Earth's loss of heat into space and therefore contribute to global temperatures through the greenhouse effect. Greenhouse gases are essential to maintaining the temperature of the Earth, however, an excess of greenhouse gases can raise the temperature of a planet to uninhabitable levels. Greenhouse gases include water vapour, carbon dioxide (CO₂), methane (CH₄), nitrous oxide (N₂O) and ozone.

Greenway

Linear green space corridors that connect natural areas and communities, associated with watercourses, trails, and transportation routes which provide wildlife habitat and increase recreational opportunities.

Green and Blue Spaces

Natural and semi-natural areas, both land and water, that are of ecological, scenic, renewable resource, outdoor recreation, and/or greenbelt value. These areas are considered to have high ecological and/or social value as green/blue spaces. Green/blue areas also includes developed, partly developed, or undeveloped public and private land.

Ground-Oriented Unit

A unit in multi-storey building that has direct access, frontage and views to the street typically through a landscaped patio, terrace and/or garden.

Healthy Communities

The Healthy Communities movement originated in Toronto in 1984, at an international conference on healthy public policy and is now globally recognized. There is no one size fits all approach to creating a healthy community, and each region has different characteristics and each community has a unique history of supporting collective health and wellbeing. Social experiences show that there are four cornerstones for success: community engagement; multi-sectoral partnerships; local government commitment; and healthy public policy.

Heritage Resource

Buildings and sites of historic, architectural, archaeological, palaeontological, or scenic significance to the District that are identified in the Heritage Register as either listed in the registry or as designated by bylaw (as per the *Local Government Act* or *Heritage Conservation Act*).

Impervious Surface

Any human-made graded, hardened surface covered with materials comprised of asphalt, concrete, masonry or combinations thereof. An impervious surface is the opposite of a permeable surface, and is much less preferred.

Infill Development

New construction or renovations which make use of vacant or underutilized parcels and which may be substantially different from the present or previous use of the parcel.

Infrastructure

The hard services associated with development including roads, railways, storm drains, water and sewer, etcetera.

Invasive Species

Plants, animals, and micro-organisms that colonize and take over the habitats of native species. Most invasive species are also non-native to the area and can become dominant because the natural controls (e.g. predators and disease) that previously kept their populations in check in their native environment no longer occur.

Landscaping

Any combination of trees, bushes, shrubs, plants, flowers, lawns, vegetation landscaping, bark mulch, decorative stones, boulders, gravel, paving, planters, foundations, sculptures, fences and the like, professionally arranged and maintained to enhance and embellish the appearance of a property or, where appropriate, to effectively screen a portion of a site.

Liveability

A measure related to quality of life which considers various amenities, services, aesthetics, opportunities and other features that impact how people live in a given place.

Mitigation

Measures taken during the planning, design, construction, and operation of works and development to alleviate potential adverse effects on natural habitats.

Mixed-Use

Developments that combine residential, commercial, and other uses in the same building or development. Residences above shops and live-work residences are examples of mixed-use developments. Mixed-use developments enable people to live close to work and amenities.

Multi-Unit Residential Building

A complex containing three or more dwelling units on a lot and includes housing typologies such as houseplexes (triplex, fourplex, etc.) townhouses, rowhouses, apartments and condominiums, also referred to as Multi-Family Development.

Multi-Modal Design

An approach to design where multiple modes of movement are incorporated into private and public developments. Linked with pedestrian-oriented and transit-oriented design approaches, multi-modal design seeks to achieve the heightened health of communities through the merging of transit, bike and car sharing, taxi and pedestrians, and to a much lesser extent, vehicles, into one integrated and inclusive system.

Municipal Campus

The collection of District of Saanich municipal buildings located east of Vernon Avenue and north of the Lochside Regional Trail and includes Saanich Municipal Hall, Saanich Police Station, and Saanich Fire Station (Fire Hall #1).

Natural Environment

Natural and semi-natural areas, both land and water, that have ecological, scenic, renewable resource, outdoor recreation, and/or greenway value. The natural environment may be within developed or undeveloped areas, whether publicly or privately owned, and not necessarily an undisturbed area.

Official Community Plan (OCP)

As set out in section 471 of the *Local Government Act*, an official community plan is a statement of objectives and policies to guide decisions on planning and land use management, within the area covered by the plan, respecting the purposes of local government. An official community plan must be adopted by bylaw. Saanich's official community plan, "Sustainable Saanich", of which this UD Plan is part, falls under Official Community Plan Bylaw, 2008, No. 8940, as amended or replaced from time to time.

Open Space

Lands that are free from buildings or structures, other than structures related to park use, and that are valued by the community for their aesthetic, recreational or ecological attributes. Lands may be in a natural state (e.g. nature parks, reserves or undevelopable lands such as flood plains, beaches and wetlands) or in a developed state (e.g. playing fields, boulevards, squares and plazas). Open spaces may be located in the public domain (e.g. municipal, regional or provincial parks, roads and pedestrian networks) or in the private domain (e.g. private plaza, internal courtyards and golf courses).

Parkland Dedication

As per the *Local Government Act*, a municipality may require a land owner to dedicate up to 5% of a parcel for park purposes for subdivision applications that propose three or more lots. A municipality can also consider cash-in-lieu for parkland dedication, where circumstances are appropriate.

Permeable Surface

A surfaced area that is porous or can be penetrated by liquid to collect precipitation and stormwater runoff (including pavers, blocks, special asphalt, etc.) thereby enabling any collection to slowly infiltrate into the soil below. A permeable surface is the opposite of an impervious surface, and is strongly encouraged.

Placemaking

An approach to the planning, design and management of public spaces that seeks to maximize the built environments function, highlighting a community's assets, inspirations and development potential, with the intention to create public spaces that promote the health, happiness, and well-being.

Publicly Accessible Open Space

An open space, often but not exclusively featuring a public amenity, to which the public is permitted access, whether it is publicly or privately owned.

Restoration

Measures taken to re-establish habitat features, functions, and conditions damaged or destroyed by human or natural activities.

Riparian Area

The moist nutrient rich lands adjacent to streams. Riparian areas are transitional zones between aquatic and terrestrial (or upland) ecosystems and often exhibit vegetation characteristics of both; they are not as dry as upland environments and not as wet as aquatic or wetland systems.

Sense of Place

The essential character and spirit of an area; a transition from space to place, providing special or unique features and fosters a sense of authentic human attachment and belonging when experienced.

Significant Street

A significant street is a street within the area of the UD Plan that is critical to the overall form and future function of the area. The principles of complete streets will be applied, and significant streets may also be subject to additional variances and unique enhancements that will set them apart from other streets in the area. Significant Streets include: Blanshard Street, Vernon Avenue, Ravine Way, Saanich Road, Boleskine Road/Harriet Road (south), Douglas Street, Oak Street and Audley Crossing (future lane).

Significant Tree

A significant tree is a tree designated as significant by bylaw, because it has one or more characteristics considered worthy of a high level of protection, such as that the tree is an outstanding specimen, a rare species, of historic significance, part of a significant row or grove, a landmark, or a wildlife habitat tree. There are more than 150 significant trees designated under Saanich's Tree Protection Bylaw, 2014, No. 9272, as amended or replaced from time to time.

Stewardship

Responsibility for the care and protection of resources so that they will be available to future generations.

Streetscape

The elements within and along the street right-of-way that define its appearance, identity, and functionality, including street furniture, public art, landscaping, trees, sidewalks and surface treatments.

Streetwall

The front wall of a building facing the street forms a streetwall. The street wall is an important urban design element that establishes human scale and contributes to the public realm. A street-wall also occurs where the sides of buildings physically touch each other and the building facades visually join together into one long wall defining a street space.

Sustainability or Sustainable Development

The concept of meeting the needs of the present without compromising the ability of future generations to meet their own needs. Sustainability is based on the efficient and environmentally responsible use of natural, human, and economic resources, the creation of efficient infrastructures and the enhancement of residents' quality of life.

Traffic Calming

Aims to reduce vehicle speeds and/or traffic to improve safety for pedestrians and cyclists, enhance quality of life for residents by reducing noise and air pollution, and recognize that streets have many social and recreational functions that can be impaired by car traffic. Examples include speed humps, lane narrowing, street trees, boulevard landscaping, chicanes and on-street parking.

Transit Oriented Development (TOD)

The practice of designing communities and planning for growth in a manner that enables and encourages people to drive less and walk, cycle and use transit more. TOD requires higher-density, mixed-use, pedestrian-oriented development in close proximity to transit. It is further reinforced by a well-connected network of streets and paths, designed with user safety and comfort in mind.

Transitional Housing

Short term housing that provides support services to help residents at risk move toward a more stable housing situation. Transitional housing may be intended for people fleeing domestic violence, people struggling with addiction, substance abuse or other mental health issues.

Universal Design

(Also called Inclusive Design, Accessible Design or Accessibility) Refers to facility designs that accommodate the widest range of potential users, including people with mobility and visual impairments and other special needs. Although universal design addresses the needs of people with disabilities, it is a comprehensive concept that can benefit all users.

Urban Design

The practice of incorporating urban planning, landscape design, engineering and architecture into the design of urban places with distinct identities, while considering developmental, political, economic and social pressures.

Urban Forest

All treed landscapes including private yards, urban parks, conservation areas, boulevards and forests within the District of Saanich.

Urban Heat Island Effect

The amplification of high temperatures in urban areas, relative to natural or rural areas, due to a greater proportion of paved surfaces, fewer trees and less vegetation, which traps and intensifies heat. The effect is further intensified by heat generation within cities, including through the increased use of furnaces, air conditioners and vehicles.

Watercourse

A river, stream, creek, waterway, lagoon, lake, spring, swamp, marsh or other natural body of fresh water; or a canal, ditch, reservoir or other man-made surface feature in which water flows constantly, intermittently or at any time.

Watershed

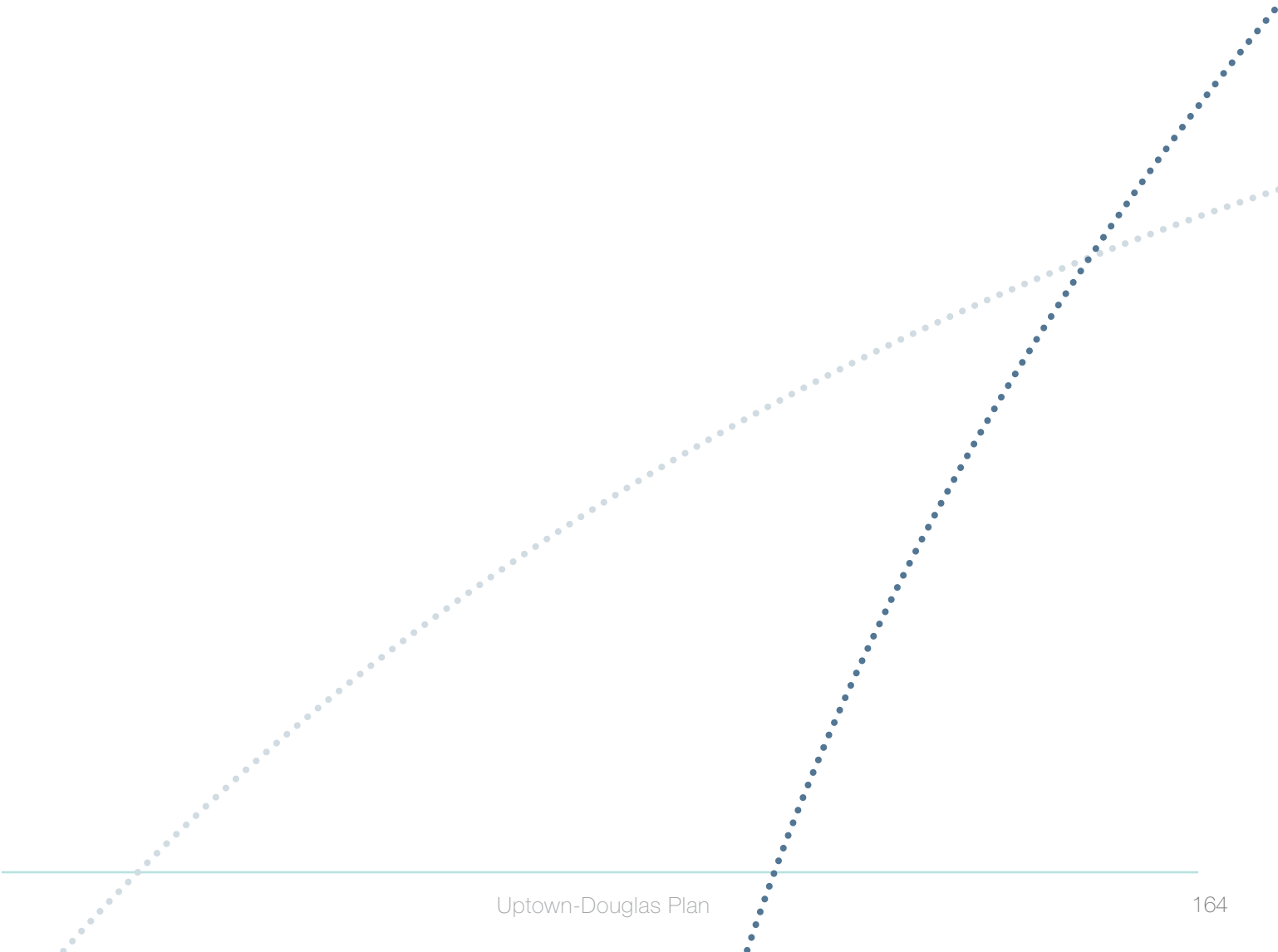
An area of land where surface water from rain, melting snow, or ice converges and sheds to a single exit-point at a body of water.

Wayfinding

A form of spatial problem solving in an urban environment. Wayfinding assists in locating and learning about one's whereabouts, both geographically and historically and in terms of the current and desired location. The combination of several elements can create good wayfinding, including signage, information/historical boards, architectural clues, lighting, banners, public art and sightlines.

Zoning

A tool used to regulate the type, size and location of uses on a property. The District of Saanich Zoning Bylaw applies a zone to every property within the municipality, regulating permitted activities, building heights, density, setbacks, parking, and other requirements related to land use.



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